

# PERMIT SITE PLAN

for

## WASHINGTON STREET SHERBORN HOMES

at

## 0 Washington Street in Sherborn, MA

### GENERAL NOTES

- ELEVATIONS SHOWN HEREON REFER TO NAVD88.
- TOPOGRAPHIC INFORMATION SHOWN HEREON WAS OBTAINED FROM SAMIOTES CONSULTANTS, INC.
- THE CONTRACTOR SHALL VERIFY THE LOCATION AND RELATIVE ELEVATION OF BENCHMARKS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION. ANY DISCREPANCY SHALL BE REPORTED TO THE ENGINEER.
- ALL SITE CONSTRUCTION SHALL COMPLY WITH THE TOWN OF SHERBORN DEPARTMENT OF PUBLIC WORKS.
- IN CASES WHERE LEDGE, BURIED FOUNDATIONS OR BOULDERS ARE PRESENT, DGT ASSOCIATES SHALL NOT BE RESPONSIBLE FOR THE AMOUNT OF ROCK OR CONCRETE ENCOUNTERED.
- DGT ASSOCIATES SHALL BE NOTIFIED OF ANY SIGNIFICANT DIFFERENCES IN THE EXISTING CONDITIONS OR UTILITIES THAT MAY AFFECT THE CONSTRUCTION SHOWN ON THIS PLAN FOR ANY NECESSARY PLAN REVISIONS.
- THIS PLAN IS NOT INTENDED TO SHOW AN ENGINEERED BUILDING FOUNDATION DESIGN WHICH WOULD INCLUDE DETAILS AND ELEVATIONS FOR FOOTINGS, FOUNDATION WALL DESIGN AND ANY SUBSURFACE DRAINAGE TO PREVENT FLOODING. COORDINATE WITH THE ARCHITECTURAL AND STRUCTURAL PLANS.
- THE PROPOSED BUILDING CONFIGURATION AS SHOWN HEREON SHALL BE CONSIDERED CONCEPTUAL AND SHALL BE VERIFIED WITH THE FINAL ARCHITECTURAL PLANS AND THE CURRENT ZONING ORDINANCES PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR AND/OR REPLACEMENT OF ANY EXISTING FEATURES DAMAGED DURING CONSTRUCTION THAT ARE NOT INTENDED FOR DEMOLITION AND/OR REMOVAL HEREON.
- SAFETY MEASURES, CONSTRUCTION METHODS AND CONTROL OF WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

### GRADING AND UTILITY PLAN NOTES

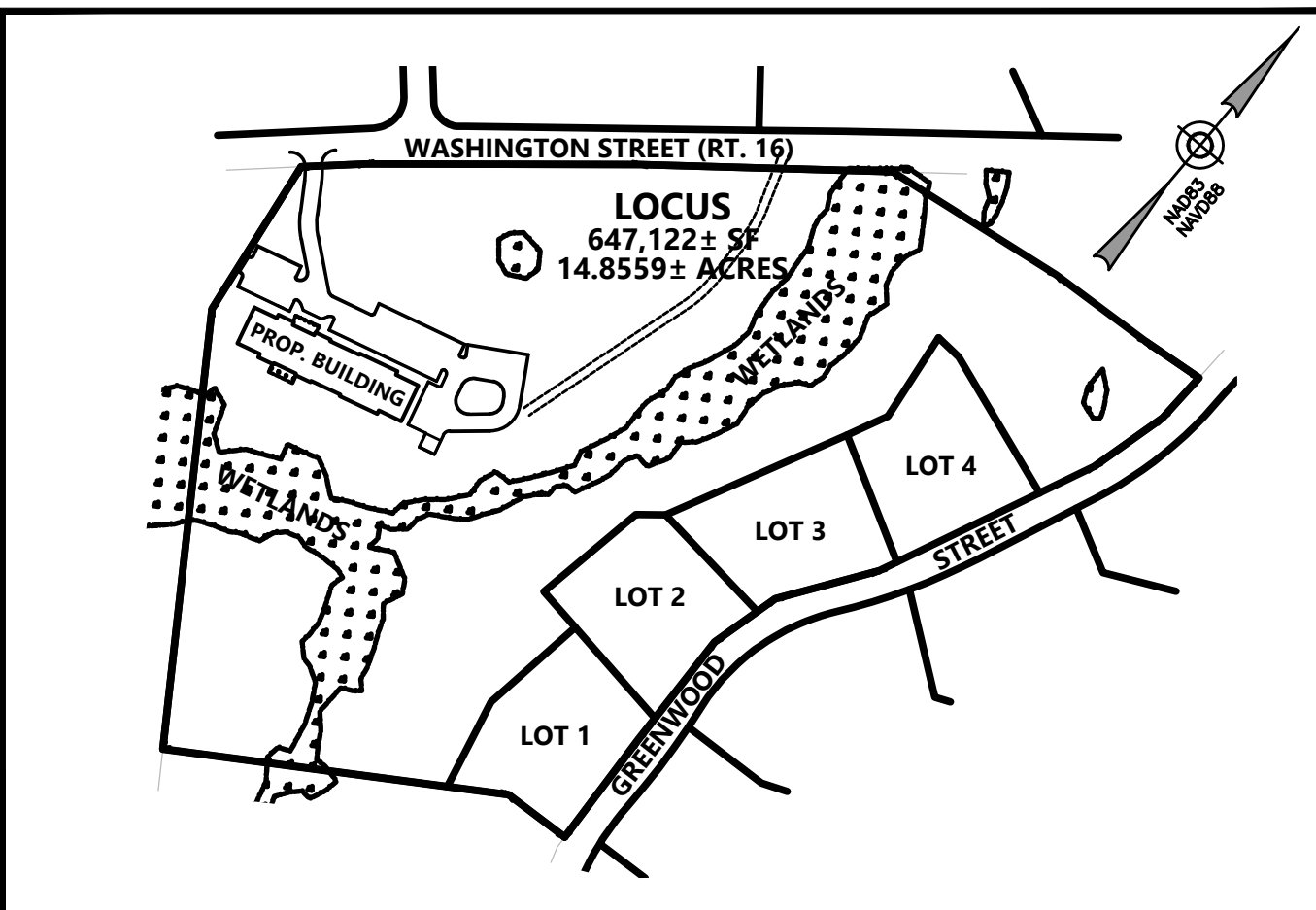
- LOCATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN HEREON ARE APPROXIMATE ONLY. ALL UTILITIES, OBSTRUCTIONS AND/OR SYSTEMS MAY NOT BE SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR LOCATING AND PROTECTING ALL EXISTING UNDERGROUND UTILITIES AND/OR SYSTEMS WHETHER OR NOT SHOWN HEREON.
- UNLESS OTHERWISE SHOWN HEREON, ALL NEW UTILITIES SHALL BE UNDERGROUND.
- RIM ELEVATIONS SHOWN HEREON FOR NEW STRUCTURES ARE PROVIDED TO ASSIST THE CONTRACTOR WITH MATERIAL TAKEOFFS. FINAL RIM ELEVATIONS SHALL MATCH PAVEMENT, GRADING, LANDSCAPING, UNLESS SPECIFICALLY INDICATED OTHERWISE.
- WHERE CONNECTIONS AND STRUCTURES ARE TO BE INSTALLED AT EXISTING UTILITIES, THE CONTRACTOR SHALL CONFIRM THE LOCATION AND ELEVATION PRIOR TO INSTALLATION AND SHALL REPORT ANY SIGNIFICANT DISCREPANCY FROM THE PLAN INFORMATION TO THE ENGINEER.
- ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED A SLOPE OF 1:50 (2%) IN ANY DIRECTION.
- WALKWAY GRADES SHALL NOT HAVE LONGITUDINAL SLOPES IN EXCESS OF 1:20 (5%) OR CROSS SLOPES IN EXCESS OF 1:50 (2%), UNLESS OTHERWISE NOTED.
- WALK GRADIENTS AND CROSS SLOPES SHOWN HEREON ARE WITHIN, OR LESS THAN, THE REQUIREMENTS OF THE MASSACHUSETTS ARCHITECTURAL ACCESS BOARD'S (MAAB) REGULATIONS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONSTRUCT THE WORK SO THAT IT IS COMPLIANT WITH ALL REQUIRED REGULATIONS. NOTE THAT THE MAXIMUM GRADIENTS WITHIN THE REGULATIONS DO NOT RECOGNIZE "CONSTRUCTION TOLERANCES." EXCEEDING THE MAXIMUM GRADIENTS IN THE REGULATIONS IS ENTIRELY AT THE CONTRACTOR'S RISK. THE CONTRACTOR SHALL VERIFY IN WRITING THAT ALL WALKS HAVE BEEN CONSTRUCTED IN COMPLIANCE WITH THE MAAB'S REGULATIONS.
- ALL ACCESSIBLE PARKING SPACES SHALL HAVE SIGNAGE AND MARKINGS COMPLIANT WITH THE MAAB REGULATIONS.
- CURB RADII AND DIMENSIONS SHOWN HEREON ARE AT THE FACE OF CURB.
- WHERE NEW PAVING MEETS EXISTING PAVING, MEET LINE AND GRADE OF EXISTING.
- AT LOCATIONS WHERE EXISTING PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING PAVEMENT SHALL BE SAWCUT TO A CLEAN AND SMOOTH EDGE.
- PERIMETER EROSION CONTROLS SHOWN HEREON SHALL BE INSTALLED PRIOR TO ANY EARTH DISTURBANCE AND SHALL SERVE AS A LIMIT OF WORK, UNLESS OTHERWISE SHOWN. THE CONTRACTOR SHALL PROVIDE DUST CONTROL FOR CONSTRUCTION OPERATIONS AS APPROVED BY THE ENGINEER.
- ALL POINTS OF CONSTRUCTION EGRESS OR INGRESS SHALL BE MAINTAINED TO PREVENT THE TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC ROADS.

### REGULATORY NOTES

- THE LOCATION OF ALL UNDERGROUND UTILITIES SHOWN HEREON ARE BASED ON FIELD LOCATION OF VISIBLE STRUCTURES AND COMPILING INFORMATION FROM PLANS PROVIDED BY UTILITY COMPANIES AND GOVERNMENT AGENCIES. THE LOCATION SHOWN HEREON SHALL BE CONSIDERED APPROXIMATE. BEFORE ANY CONSTRUCTION, DEMOLITION OR SITE WORK, THE LOCATION OF ALL UNDERGROUND UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR BY CONTACTING "DIG-SAFE" AT 811.
- THE CONTRACTOR SHALL MAKE HIMSELF AWARE OF ALL CONSTRUCTION REQUIREMENTS, CONDITIONS AND LIMITATIONS IMPOSED BY PERMITS AND APPROVALS ISSUED BY REGULATORY AUTHORITIES PRIOR TO THE COMMENCEMENT OF ANY WORK. CONTRACTOR SHALL COORDINATE AND OBTAIN ALL CONSTRUCTION PERMITS REQUIRED BY REGULATORY AUTHORITIES.
- ALL WORK OUTSIDE OF THE BUILDING THAT IS LESS THAN 10 FEET FROM THE INSIDE FACE OF THE BUILDING FOUNDATIONS SHALL CONFORM WITH THE UNIFORM STATE PLUMBING CODE OF MASSACHUSETTS, 248 CMR.
- CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE RULES AND REGULATIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA).

### SHEET INDEX

- |      |  |
|------|--|
| C-1  | TITLE SHEET                                    |
| C-2  | EROSION AND SEDIMENT CONTROL PLAN              |
| C-3  | LAYOUT AND MATERIALS PLAN                      |
| C-4  | SITE GRADING PLAN                              |
| C-5  | DRAINAGE PLAN                                  |
| C-6  | SITE UTILITIES PLAN                            |
| C-7  | EROSION AND SEDIMENT CONTROL NOTES AND DETAILS |
| C-8  | SITE DETAILS - 01                              |
| C-9  | SITE DETAILS - 02                              |
| C-10 | SITE DETAILS - 03                              |
| C-11 | SITE DETAILS - 04                              |
| C-12 | SITE DETAILS - 05                              |



SITE LOCUS MAP

1" = 250'

Table 1. Summary of Project Site Conditions

General Site Conditions	Land Condition	Land Breakdown	Acres	Square Feet	Coverage (%)
	Unusable Land	Wetlands	2.5762	112,221	17.3%
	Usable Land	Upland	12.2796	534,901	82.7%
Existing Conditions	Disturbed	Total	0.0000	0	0.0%
	Impervious	Building	0.0000	0	0.0%
		Pavement	0.0000	0	0.0%
		Sidewalk	0.0000	0	0.0%
	Pervious	Lawn (usable open space)	0.0000	0	0.0%
	Undisturbed	Total	14.8559	647,122	100.0%
	Usable Open Space	Upland Woods	12.2796	534,901	82.7%
	Unusable Open Space	Wetlands	2.5762	112,221	17.3%
	Total Usable Open Space	Lawn / Woods	12.2796	534,901	82.7%
	Disturbed	Total	2.5493	111,047	17.2%
Proposed Conditions	Impervious	Building	0.3323	14,475	2.2%
		Pavement	0.6955	30,296	4.7%
		Sidewalk	0.0649	2,826	0.4%
	Pervious	Lawn (usable open space)	1.4566	63,450	9.8%
	Undisturbed	Total	12.3066	536,075	82.8%
	Usable Open Space	Upland Woods	9.7303	423,854	65.5%
	Unusable Open Space	Wetlands	2.5762	112,221	17.3%
	Total Usable Open Space	Lawn / Woods	11.1870	487,304	75.3%
	Disturbed	Total	2.5493	111,047	17.2%
	Impervious	Building	0.3323	14,475	2.2%

Table 2. Summary of Proposed Building

Item	Total	1 Bedroom	2 Bedroom	3 Bedroom
Units	40	14	22	4
Bedrooms	70	14	44	12

Table 3. Zoning Summary Table (Sherborn Residential B District)

Item	Required	Proposed Conditions	Waiver Required
Minimum Lot Area	2 acres	14.8559 acres	No
Minimum Frontage	200 feet	792.4 feet	No
Minimum Lot Width	200 feet	792.4 feet	No
Minimum Lot Depth	N/A	N/A	N/A
Minimum Front Setback	60 feet	197.7 feet	No
Minimum Side Setback	40 feet	39.0 feet	Yes
Minimum Rear Setback	30 feet	301.8 feet	No
Maximum Height (stories)	2.5 stories	3 stories	Yes
Maximum Height (feet)	35 feet	> 35 feet	Yes
Maximum Lot Coverage	N/A	N/A	N/A
Proposed Surface Cover			
Building Footprint	2.2%		
Parking / Paved Areas	5.1%		
Total Impervious Area	7.3%		
Unusable Open Space (wetlands)	17.3%		
Usable Open Space	75.3%		

PARCEL ID:

MAP 7, LOT 0, BLOCK 49

ISSUED FOR:

COMPREHENSIVE  
PERMIT APPLICATION



1	BEC	2/9/24	DESIGN DEVELOPMENT
NO.	APP	DATE	DESCRIPTION

DATE: JANUARY 23, 2024

SCALE: AS NOTED

DESIGN: KMR	DRAFTED: KMR/JAL	CHECKED: JAL/BEC
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PROJECT TITLE:

WASHINGTON  
STREET  
SHERBORN  
HOMES

0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770

SHEET TITLE:

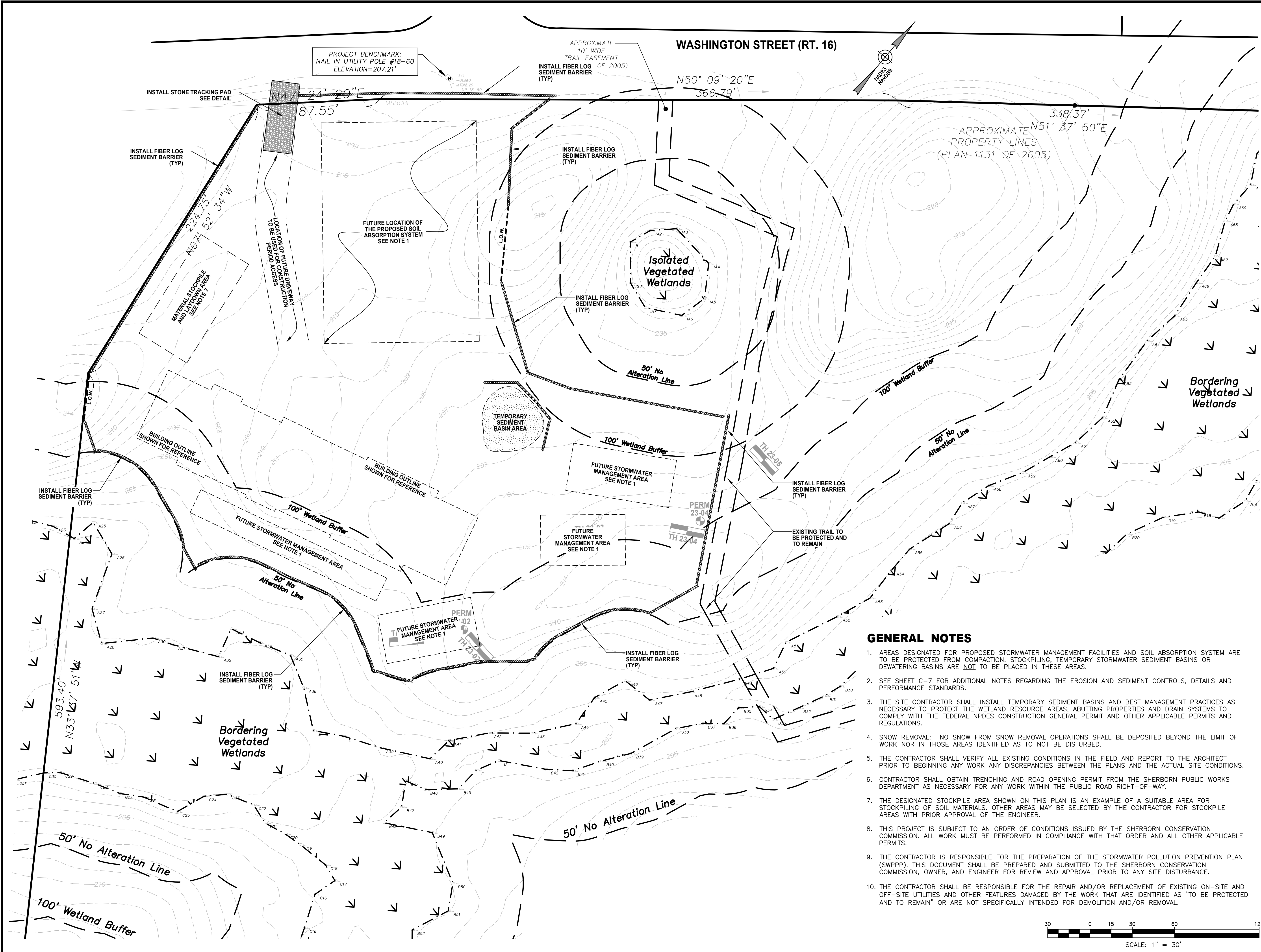
TITLE SHEET

SHEET:  
1 OF 12

PROJECT NO.:  
F-25902

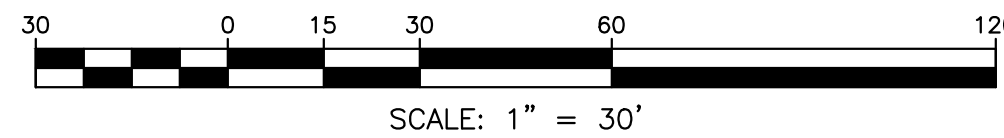
C-1





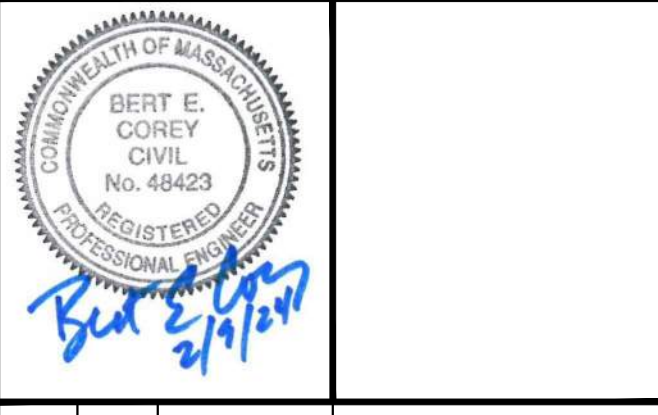
**GENERAL NOTES**

1. AREAS DESIGNATED FOR PROPOSED STORMWATER MANAGEMENT FACILITIES AND SOIL ABSORPTION SYSTEM ARE TO BE PROTECTED FROM COMPACTION, STOCKPILING, TEMPORARY STORMWATER SEDIMENT BASINS OR DEWATERING BASINS ARE **NOT** TO BE PLACED IN THESE AREAS.
2. SEE SHEET C-7 FOR ADDITIONAL NOTES REGARDING THE EROSION AND SEDIMENT CONTROLS, DETAILS AND PERFORMANCE STANDARDS.
3. THE SITE CONTRACTOR SHALL INSTALL TEMPORARY SEDIMENT BASINS AND BEST MANAGEMENT PRACTICES AS NECESSARY TO PROTECT THE WETLAND RESOURCE AREAS, ABUTTING PROPERTIES AND DRAIN SYSTEMS TO COMPLY WITH THE FEDERAL NPDES CONSTRUCTION GENERAL PERMIT AND OTHER APPLICABLE PERMITS AND REGULATIONS.
4. SNOW REMOVAL: NO SNOW FROM SNOW REMOVAL OPERATIONS SHALL BE DEPOSITED BEYOND THE LIMIT OF WORK NOR IN THOSE AREAS IDENTIFIED AS TO NOT BE DISTURBED.
5. THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS IN THE FIELD AND REPORT TO THE ARCHITECT PRIOR TO BEGINNING ANY WORK ANY DISCREPANCIES BETWEEN THE PLANS AND THE ACTUAL SITE CONDITIONS.
6. CONTRACTOR SHALL OBTAIN TRENCHING AND ROAD OPENING PERMIT FROM THE SHERBORN PUBLIC WORKS DEPARTMENT AS NECESSARY FOR ANY WORK WITHIN THE PUBLIC ROAD RIGHT-OF-WAY.
7. THE DESIGNATED STOCKPILE AREA SHOWN ON THIS PLAN IS AN EXAMPLE OF A SUITABLE AREA FOR STOCKPILING OF SOIL MATERIALS. OTHER AREAS MAY BE SELECTED BY THE CONTRACTOR FOR STOCKPILE AREAS WITH PRIOR APPROVAL OF THE ENGINEER.
8. THIS PROJECT IS SUBJECT TO AN ORDER OF CONDITIONS ISSUED BY THE SHERBORN CONSERVATION COMMISSION. ALL WORK MUST BE PERFORMED IN COMPLIANCE WITH THAT ORDER AND ALL OTHER APPLICABLE PERMITS.
9. THE CONTRACTOR IS RESPONSIBLE FOR THE PREPARATION OF THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THIS DOCUMENT SHALL BE PREPARED AND SUBMITTED TO THE SHERBORN CONSERVATION COMMISSION, OWNER, AND ENGINEER FOR REVIEW AND APPROVAL PRIOR TO ANY SITE DISTURBANCE.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR AND/OR REPLACEMENT OF EXISTING ON-SITE AND OFF-SITE UTILITIES AND OTHER FEATURES DAMAGED BY THE WORK THAT ARE IDENTIFIED AS "TO BE PROTECTED AND TO REMAIN" OR ARE NOT SPECIFICALLY INTENDED FOR DEMOLITION AND/OR REMOVAL.



PARCEL ID:  
**MAP 7, LOT 0, BLOCK 49**

ISSUED FOR:  
**COMPREHENSIVE PERMIT APPLICATION**



NO.	APP	DATE	DESCRIPTION
1	BEC	2/9/24	DESIGN DEVELOPMENT

DATE: **JANUARY 23, 2024**

SCALE: **1" = 30'**

DESIGN: <b>KMR</b>	DRAFTED: <b>KMR/JAL</b>	CHECKED: <b>JAL/BEC</b>
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PROJECT TITLE:

**WASHINGTON STREET  
SHERBORN HOMES**

**0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770**

SHEET TITLE:

**EROSION AND  
SEDIMENT CONTROL  
PLAN**

SHEET: <b>2 OF 12</b>	<b>C-2</b>
PROJECT NO.: <b>F-25902</b>	

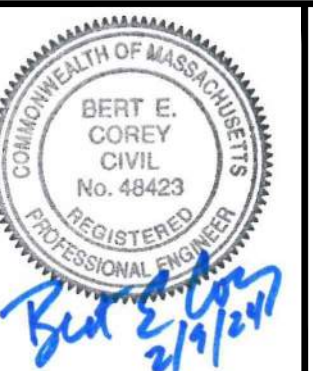


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**MAP 7, LOT 0, BLOCK 49**

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PERMIT APPLICATION**



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KMR	KMR/JAL	JAL/BEC

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**WASHINGTON  
STREET  
SHERBORN  
HOMES**

**0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770**

SHEET TITLE:

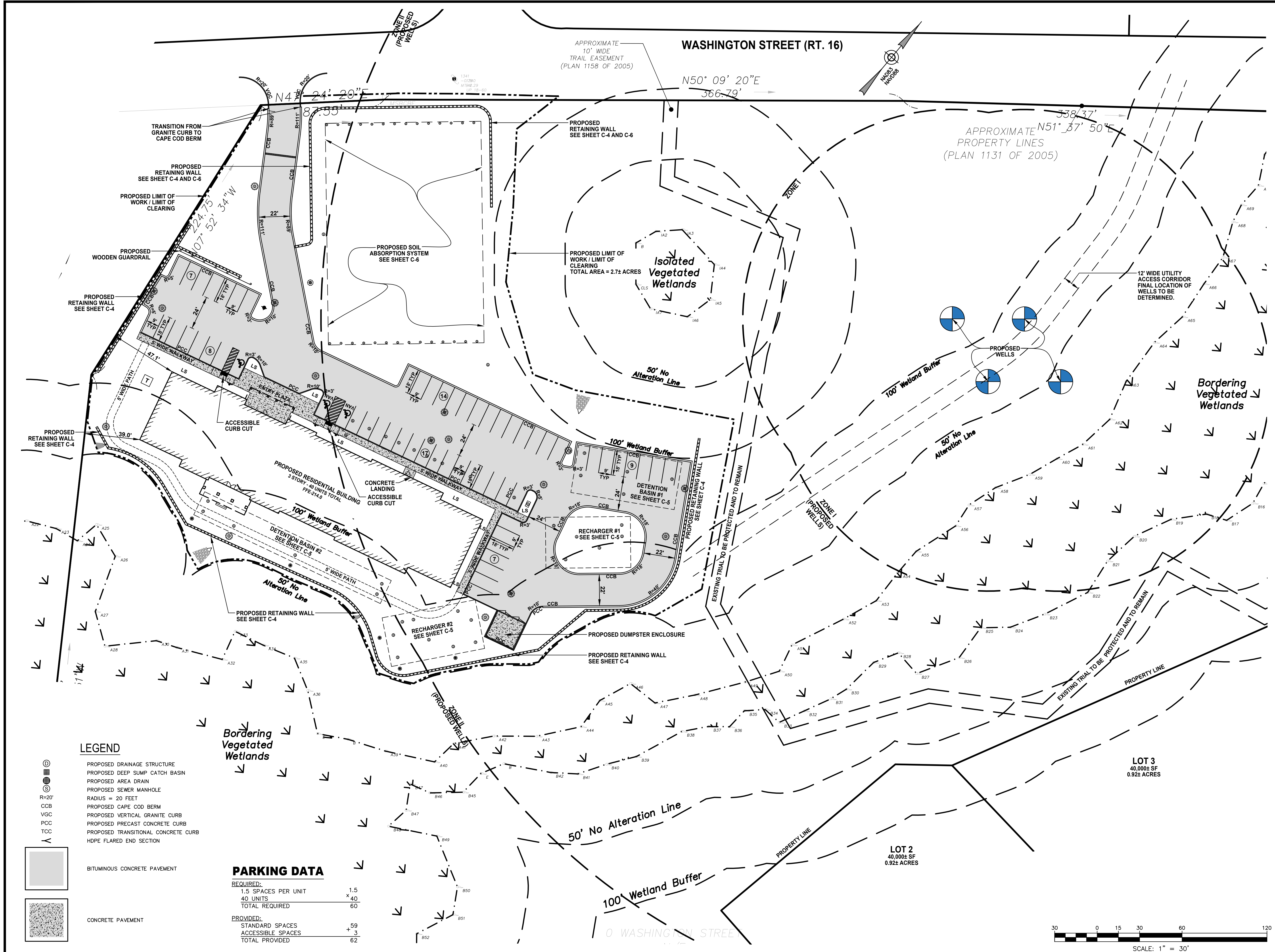
**LAYOUT AND  
MATERIALS PLAN**

SHEET:  
**3 OF 12**

PROJECT NO.:  
**F-25902**

**C-3**

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**LEGEND**

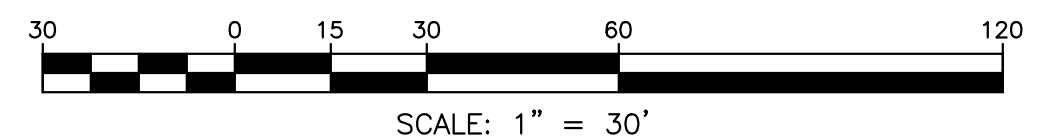
- PROPOSED DRAINAGE STRUCTURE
- PROPOSED DEEP SUMP CATCH BASIN
- PROPOSED AREA DRAIN
- PROPOSED SOWER MANHOLE
- RADIUS = 20 FEET
- PROPOSED CAPE COD BERM
- PROPOSED VERTICAL GRANITE CURB
- PROPOSED PRECAST CONCRETE CURB
- PROPOSED TRANSITIONAL CONCRETE CURB
- HDPE FLARED END SECTION

BITUMINOUS CONCRETE PAVEMENT

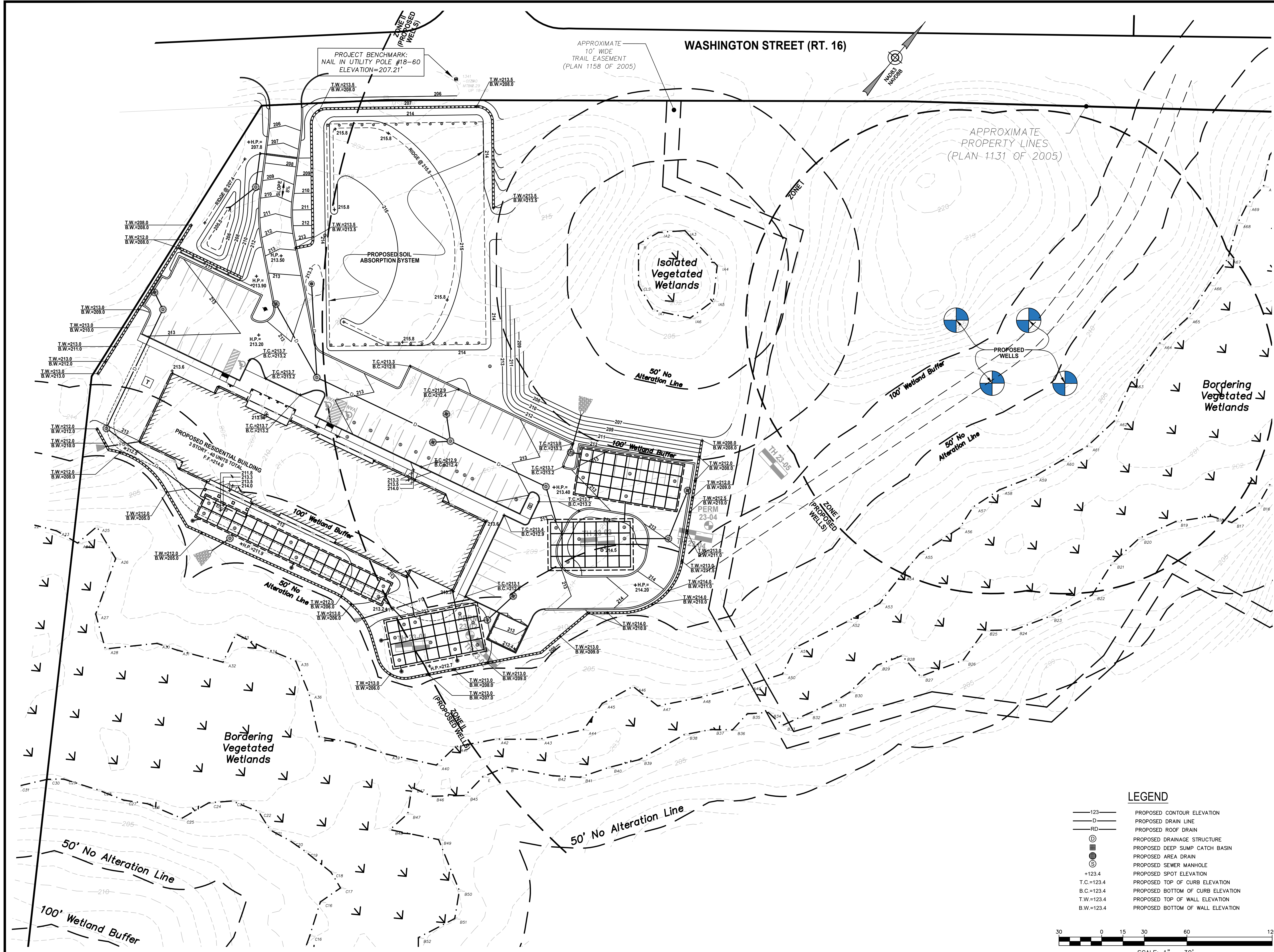
CONCRETE PAVEMENT

**PARKING DATA**

REQUIRED:	
1.5 SPACES PER UNIT	1.5
40 UNITS	x 40
TOTAL REQUIRED	60
PROVIDED:	
STANDARD SPACES	59
ACCESSIBLE SPACES	+ 3
TOTAL PROVIDED	62

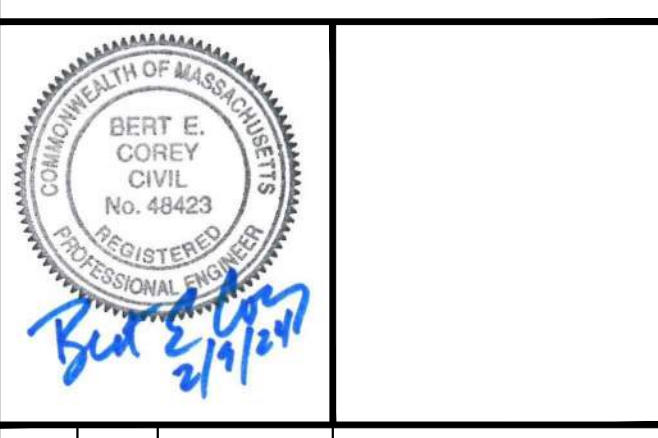






PARCEL ID:  
**MAP 7, LOT 0, BLOCK 49**

ISSUED FOR:  
**COMPREHENSIVE PERMIT APPLICATION**



1	BEC	2/9/24	DESIGN DEVELOPMENT
NO.	APP	DATE	DESCRIPTION

DATE: **JANUARY 23, 2024**

SCALE: **1" = 30'**

DESIGN:	DRAFTED:	CHECKED:
KMR	KMR/JAL	JAL/BEC

PROJECT TITLE:  
**WASHINGTON STREET SHERBORN HOMES**  
**0 WASHINGTON STREET SHERBORN, MASSACHUSETTS 01770**  
SHEET TITLE:

**SITE GRADING PLAN**



PARCEL ID:

MAP 7, LOT 0, BLOCK 49

ISSUED FOR:

COMPREHENSIVE  
PERMIT APPLICATION



NO.	APP	DATE	DESCRIPTION
1	BEC	2/9/24	DESIGN DEVELOPMENT

DATE: JANUARY 23, 2024

SCALE: 1" = 30'

DESIGN:	DRAFTED:	CHECKED:
KMR	KMR/JAL	JAL/BEC

PROJECT TITLE:

**WASHINGTON  
STREET  
SHERBORN  
HOMES**

0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770

SHEET TITLE:

**DRAINAGE PLAN**

SHEET:  
5 OF 12

PROJECT NO.:  
F-25902

**C-5**

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WASHINGTON STREET (RT. 16)

APPROXIMATE  
10' WIDE  
TRAIL EASEMENT  
(PLAN 1158 OF 2005)

N50° 09' 20"E  
366.79'

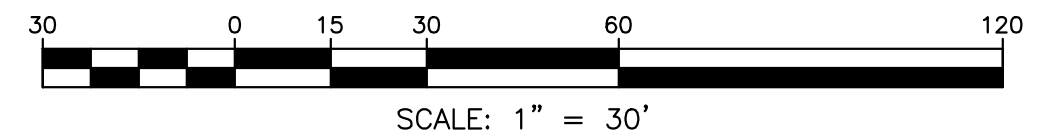
APPROXIMATE  
PROPERTY LINES  
(PLAN 1131 OF 2005)

Isolated  
Vegetated  
Wetlands

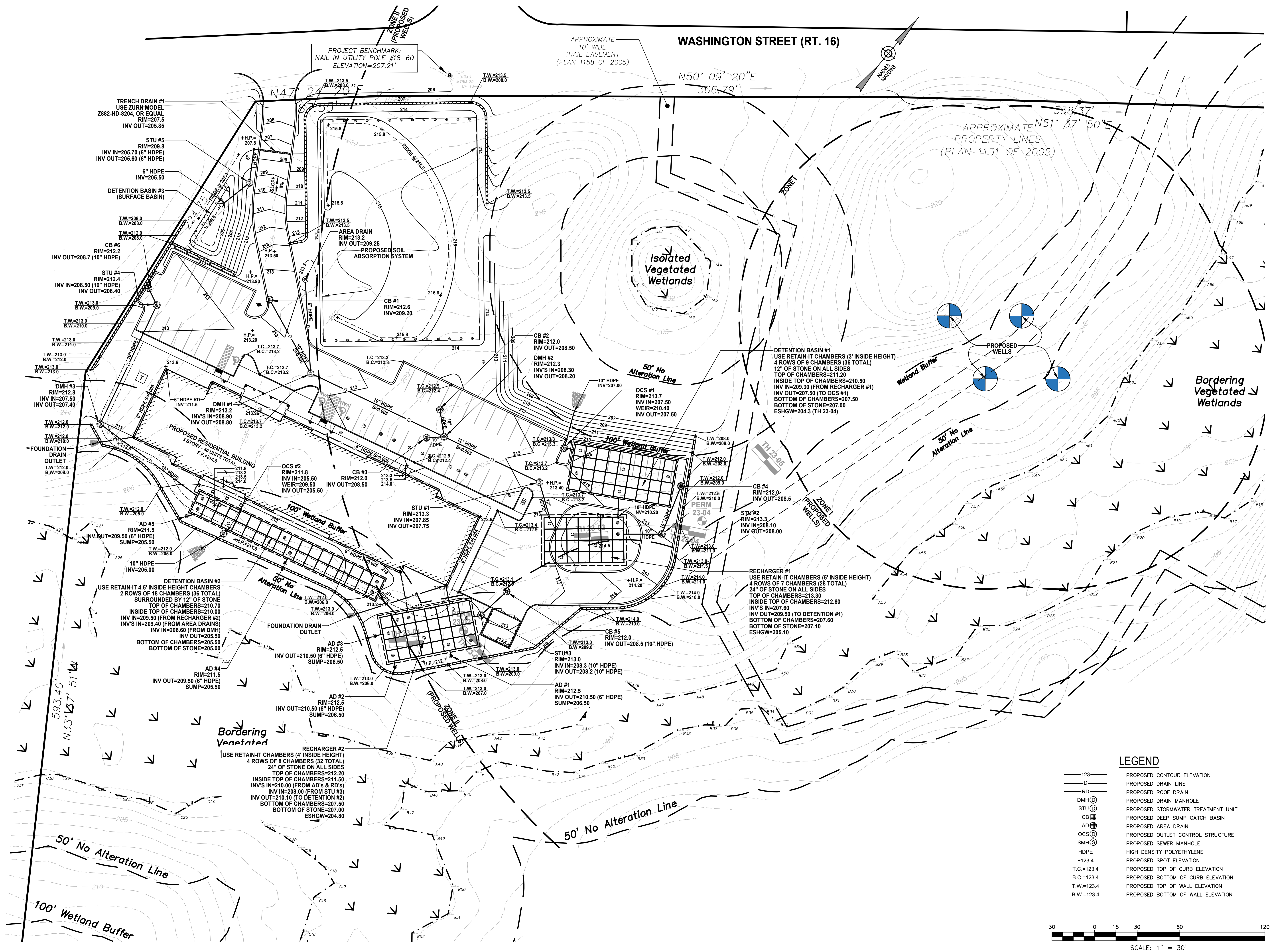
Bordering  
Vegetated  
Wetlands

**LEGEND**

123	PROPOSED CONTOUR ELEVATION
D	PROPOSED DRAIN LINE
RD	PROPOSED ROOF DRAIN
DMH	PROPOSED DRAIN MANHOLE
STU	PROPOSED STORMWATER TREATMENT UNIT
CB	PROPOSED DEEP SUMP CATCH BASIN
AD	PROPOSED AREA DRAIN
OCS	PROPOSED OUTLET CONTROL STRUCTURE
SMH	PROPOSED SEWER MANHOLE
HDPE	HIGH DENSITY POLYETHYLENE
+123.4	PROPOSED SPOT ELEVATION
T.C.=123.4	PROPOSED TOP OF CURB ELEVATION
B.C.=123.4	PROPOSED BOTTOM OF CURB ELEVATION
T.W.=123.4	PROPOSED TOP OF WALL ELEVATION
B.W.=123.4	PROPOSED BOTTOM OF WALL ELEVATION



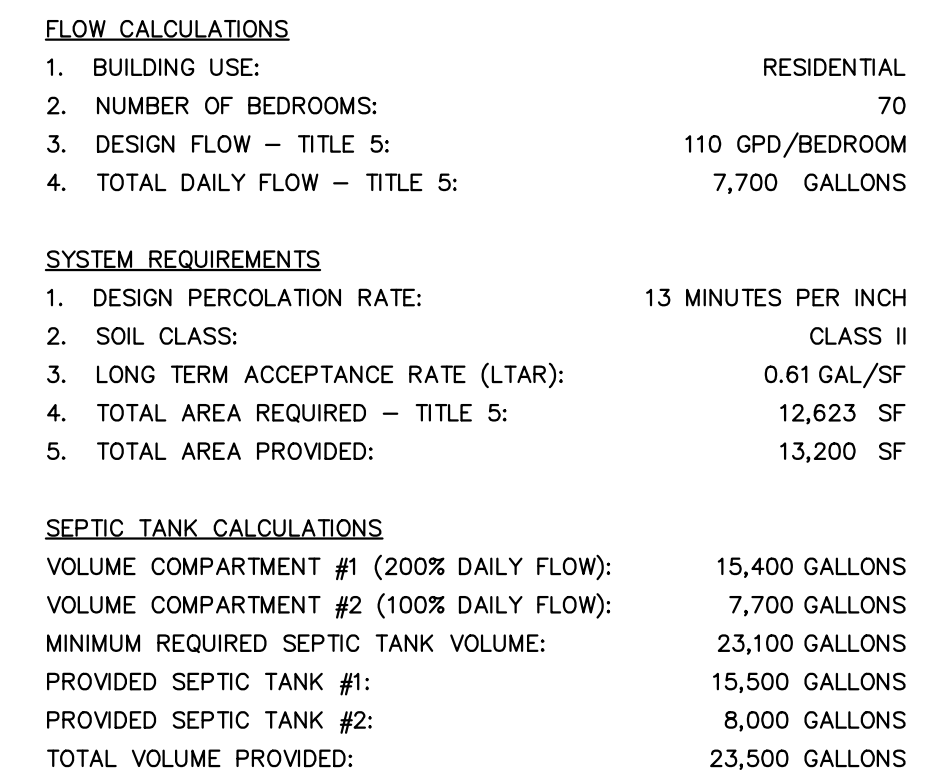
SCALE: 1" = 30'







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# EROSION AND SEDIMENT CONTROL NOTES

## 1. CONSTRUCTION PERIOD EROSION AND SEDIMENT CONTROL

1.1 THE PURPOSE OF THE CONSTRUCTION PERIOD EROSION AND SEDIMENT CONTROL PLAN IS TO MINIMIZE THE INTRODUCTION OF SEDIMENTS ONTO PUBLIC RIGHT OF WAYS, ADJUTING PROPERTIES, AND TO POST-DEVELOPMENT STORMWATER BMP'S RESULTING FROM THE LAND DISTURBANCE ACTIVITIES DURING CONSTRUCTION.

1.2 INSPECTIONS SHALL BE CONDUCTED BY THE GENERAL CONTRACTOR ON A BI-WEEKLY BASIS (EVERY TWO WEEKS), OR FOLLOWING SIGNIFICANT STORM EVENTS (RAINFALL OF 0.5" OR MORE) THAT CAN AFFECT THE SEDIMENT AND EROSION CONTROL PRACTICES IMPLEMENTED AT THE SITE. THE PURPOSE OF THE INSPECTIONS IS TO EVALUATE THE EFFECTIVENESS OF THE CONTROLS AND ANY REQUIRED MAINTENANCE ACTIVITIES. IF AN EROSION/SEDIMENTATION CONTROL MEASURE IS FOUND TO BE INADEQUATE FOR PROPERLY CONTROLLING SEDIMENT, AN ADEQUATE MEASURE SHALL BE DESIGNED AND IMPLEMENTED. A COPY OF THE WRITTEN INSPECTION SHALL BE KEPT ON FILE AT THE CONSTRUCTION SITE.

1.3 DURING CONSTRUCTION, PROPOSED STORMWATER MANAGEMENT STRUCTURES SHALL BE PROTECTED FROM SEDIMENT. ALL PROPOSED NEW STORMWATER MANAGEMENT STRUCTURES THAT INFILTRATE RUNOFF ARE PARTICULARLY SENSITIVE TO DAMAGE BY SEDIMENT. INFILTRATION TECHNOLOGIES ARE NOT DESIGNED TO HANDLE THE HIGH CONCENTRATIONS OF SEDIMENTS TYPICALLY FOUND IN CONSTRUCTION RUNOFF, AND MUST BE PROTECTED FROM CONSTRUCTION RELATED SEDIMENT LOADINGS. SITE RUNOFF FROM UNSTABILIZED AREAS SHALL NOT BE DISCHARGED INTO THE PROPOSED INFILTRATION SYSTEMS, TRENCH DRAIN, AND CATCH BASINS UNTIL THE TRIBUTARY DRAINAGE AREA IS STABLE OR THE RUNOFF IS TREATED TO BE ESSENTIALLY FREE FROM SEDIMENT TO THE SATISFACTION OF THE ENGINEER. THE CONTRACTOR SHALL PROVIDE TEMPORARY BY-PASS SYSTEMS AS NECESSARY TO PREVENT CONSTRUCTION SITE RUNOFF FROM ENTERING THE INFILTRATION SYSTEMS. THE INFILTRATION SYSTEMS SHALL REMAIN OFF-LINE AND PROTECTED. CLEAN ROOF RUNOFF MAY DISCHARGE INTO THE INFILTRATION SYSTEMS IF IT IS PIPED DIRECTLY TO THE SYSTEMS AND NOT DIRECTED OVER DISTURBED AREAS.

1.4 NO STOCKPILING IS ALLOWED WITHIN THE FOOTPRINT OF THE PROPOSED INFILTRATION SYSTEMS OR THE FOOTPRINT OF THE PROPOSED SOIL ABSORPTION FIELD. CONTRACTOR IS TO LOCATE THE PROPOSED AREAS FOR THESE SYSTEMS PRIOR TO THE START OF CONSTRUCTION.

1.5 DURING THE INSTALLATION OF THE INFILTRATION SYSTEMS, NO PARKING WILL BE PERMITTED OVER THE SYSTEM UNTIL IT HAS BEEN PROPERLY BACKFILLED AND PROTECTED FROM SEDIMENT AND VEHICULAR LOADS.

1.6 NO PARKING IS ALLOWED OVER THE FOOTPRINT OF THE PROPOSED SOIL ABSORPTION SYSTEM AT ANYTIME DURING THE CONSTRUCTION PROCESS.

## 2. GENERAL PERFORMANCE STANDARDS:

2.1 THE CONTRACTOR SHALL INSTALL, ROUTINELY INSPECT AND MAINTAIN ALL SEDIMENT AND EROSION CONTROLS SUCH THAT THEY ARE IN PROPER WORKING ORDER AT ALL TIMES DURING THE CONSTRUCTION PROJECT UNTIL SUCH TIME AS ALL AREAS OF THE SITE TRIBUTARY TO THOSE EROSION CONTROLS ARE IN A PERMANENTLY STABILIZED CONDITION.

2.2 THE CONTRACTOR SHALL MANAGE THE SITE SUCH THAT EROSION AND SEDIMENT FROM RUNOFF AND WIND BLOWN DUST ARE CONTROLLED AND MINIMIZED AT ALL TIMES. THE EROSION CONTROLS SHOWN ON THIS PLAN INCLUDE THE INITIAL SETUP OF EROSION CONTROLS AND BASIC INFORMATION TO MEET THE REQUIREMENT OF BEST MANAGEMENT PRACTICES. THE CONTRACTOR MUST MANAGE THE SITE PROPERLY WHICH MAY INCLUDE, BUT NOT BE LIMITED TO: MINIMIZING AREAS OF EXPOSED SOILS; INSTALLING TEMPORARY COVER; MAKE NECESSARY ADJUSTMENTS TO THE EROSION CONTROL INSTALLATIONS TO IMPROVE FUNCTION; PROVIDE TEMPORARY SEDIMENT BASINS; INSTALL ADDITIONAL EROSION CONTROL WHERE NECESSARY.

2.3 THE EROSION CONTROL WORK SHOWN ON THIS PLAN ARE ALSO SUBJECT TO OTHER STATE AND LOCAL APPROVALS. THE CONTRACTOR SHALL OBTAIN AND BE RESPONSIBLE FOR COMPLIANCE WITH THE CONDITIONS AND REQUIREMENTS OF THOSE PERMITS.

2.4 DESIGN, INSTALLATION AND MAINTENANCE OF SEDIMENT AND EROSION CONTROLS SHALL BE IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES FOLLOWING THE GUIDELINES INCLUDE IN THE FOLLOWING:

- "STORMWATER MANAGEMENT FOR CONSTRUCTION ACTIVITIES, DEVELOPING POLLUTION PREVENTION PLANS AND BEST MANAGEMENT PRACTICES" U.S. ENVIRONMENTAL PROTECTION AGENCY, OCTOBER 1992.
- "MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES FOR URBAN AND SUBURBAN AREAS, A GUIDE FOR PLANNERS, DESIGNERS AND MUNICIPAL OFFICIALS", MASS. EXECUTIVE OFFICE OF ENVIRONMENTAL AFFAIRS, MAY 2003.
- U.S.D.A. NATURAL RESOURCES AND CONSERVATION SERVICES (NRCS) GUIDELINES.

## 3. FEDERAL NPDES PHASE II COMPLIANCE:

3.1 THIS PROJECT IS SUBJECT TO THE FEDERAL CLEAN WATER ACT REQUIREMENTS FOR CONSTRUCTION SITES ADMINISTERED BY THE U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA). THIS PROGRAM IS THE "NATIONAL POLLUTANT DISCHARGE ELIMINATION (NPDES) - PHASE II FOR CONSTRUCTION SITES. FOR COMPLIANCE WITH THIS PROGRAM, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A COMPLETE "STORMWATER POLLUTION PREVENTION PLAN" (SWPPP) IN CONFORMANCE WITH THE CURRENT CONSTRUCTION GENERAL PERMIT AND FILING A NOTICE OF INTENT WITH THE EPA UNDER THE NPDES PROGRAM. THE CONTRACTOR SHALL THEN MANAGE THE SITE IN COMPLIANCE WITH THE SWPPP AND IN COMPLIANCE WITH THE CONSTRUCTION GENERAL PERMIT. NOTE THAT THE SWPPP IS TO BE CONSISTENT WITH THIS EROSION AND SEDIMENT CONTROL PLAN AND OTHER APPLICABLE APPROVALS. THE EROSION AND SEDIMENT CONTROL PLAN INCLUDED IN THIS PLAN SET MAY BE USED AS PART OF THE DOCUMENTATION REQUIRED FOR THE PREPARATION OF A SWPPP, BUT IS NOT TO BE CONSIDERED AS MEETING THE FULL REQUIREMENTS OF A SWPPP PREPARED FOR COMPLIANCE WITH THE NPDES PROGRAM.

## 4. PERIMETER FIBER LOG / SILT FENCING AND LIMIT OF WORK:

4.1 PRIOR TO ANY DISTURBANCE OR ALTERATIONS OF ANY AREA ON THE SITE, AN EROSION CONTROL BARRIER AND CONSTRUCTION FENCE SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PLAN.

4.2 INSTALL THE FIBER LOGS ON THE UPHILL SIDE OF THE SILT FENCE AS SHOWN. IN THOSE AREAS WHERE THE TOPOGRAPHY INDICATES THAT STORMWATER RUNOFF WILL BE CONCENTRATED (AT LOW POINTS), ADDITIONAL FIBER LOGS, AS NECESSARY, SHALL BE STAKED ON THE UPHILL SIDE OF THE SEDIMENT BARRIER FOR ADDED FILTRATION AND PROTECTION. THE REQUIRED LOCATIONS FOR THE ADDED FIBER LOG INSTALLATION WILL BE SELECTED BY THE ENGINEER AND / OR THE AUTHORIZED INSPECTOR / SWPPP COORDINATOR UPON COMPLETION OF THE SEDIMENT BARRIER INSTALLATION. (SEE DETAIL).

4.3 ONCE INSTALLED, THE STAKED FIBER LOGS AND CONSTRUCTION FENCE SHALL BE MAINTAINED IN PLACE UNTIL ALL AREAS UPGRADEMENT FROM THE BARRIERS HAVE BEEN PERMANENTLY STABILIZED.

4.4 ALL DISTURBED AREAS NOT OTHERWISE DEVELOPED OR WHERE SPECIAL STABILIZATION MEASURES OR LANDSCAPE PLANTINGS ARE PROPOSED SHALL BE LOAMED AND SEEDED OR SODDED. SIX INCHES OF LOAM TOPSOIL (MIN. COMPACTED DEPTH) SHALL BE APPLIED UNLESS, OTHERWISE SPECIFIED. (SEE LANDSCAPE PLAN AND OTHER PLANS AS APPLICABLE).

4.5 THE PERIMETER FIBER LOGS AND CONSTRUCTION FENCE ARE ALSO LIMITS OF WORK. ALL AREAS OUTSIDE THE LIMIT ARE TO BE LEFT UNDISTURBED. DURING THE SITE WORK, ALL PERSONS AND EQUIPMENT SHALL STAY OUT OF THESE AREAS TO PRESERVE THE EXISTING VEGETATION AND SOIL COVER.

## 5. CONSTRUCTION ENTRANCE:

5.1 AT THE START OF SITE WORK, A STONE CONSTRUCTION ENTRANCE SHALL BE INSTALLED AT THE ACCESS TO THE SITE FROM THE ACCESS EASEMENT DRIVEWAY TO CONTROL THE TRACKING OF MUD OFF THE SITE. THE ENTRANCE SHALL BE MAINTAINED UNTIL THE SITE IS IN A STABILIZED CONDITION WHEN THE POSSIBILITY OF VEHICLES TRACKING MUD OFF SITE HAS BEEN ELIMINATED.

5.2 THE CONTRACTOR SHALL SWEEP THE ADJACENT ROADWAYS WHEN MUD, DUST, DIRT, DEBRIS, ETC. HAS SHOWN SIGNS OF BUILDUP ON THE ROADWAYS EXITING THE SITE. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO THIS MATTER AND IMMEDIATE ATTENTION IS ALWAYS REQUIRED.

## 6. DEWATERING OF EXCAVATIONS:

6.1 DISCHARGE FROM DEWATERING PUMPS OR TEMPORARY TRENCH OR EXCAVATION DRAINS SHALL NOT BE DISCHARGED DIRECTLY TO THE ON-SITE DRAINAGE SYSTEM, WETLANDS, OR SEWAGE DISPOSAL SYSTEM. DISCHARGES SHALL BE DIRECTED TO A TREATMENT SYSTEM CONSISTING OF A SEDIMENT BASIN, STRAW BALE SEDIMENT BASIN, FILTER BAG SYSTEM OR OTHER APPROVED METHOD TO FILTER THE DISCHARGE WATER AND PREVENT EROSION.

6.2 ALL DEWATERING DRAINAGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES SHALL COMPLY WITH THE TOWN OF SHERBORN REQUIREMENTS, UNITED STATES ENVIRONMENTAL PROTECTION AGENCY, MASSACHUSETTS DEP AND OTHER APPROPRIATE AGENCIES. UNDER NO CIRCUMSTANCE SHALL DEWATERING DRAINAGE BE DISCHARGED INTO SANITARY SEWER.

## 7. SOIL STOCKPILES:

7.1 STOCKPILES OF SOIL MATERIALS SHALL BE PLACED WITHIN AREAS THAT ARE PROTECTED BY PERIMETER EROSION CONTROLS, OR SHALL BE SURROUNDED BY PROPER SILT FENCE OR FIBER LOGS.

7.2 STOCKPILES THAT ARE TO BE IN PLACE FOR EXTENDED PERIODS OF TIME (MORE THAN 30 DAYS) SHALL BE COVERED OR OTHERWISE TEMPORARILY STABILIZED IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES.

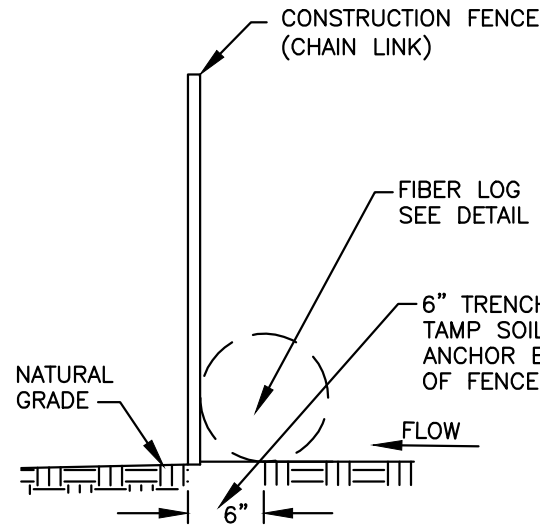
## 8. CATCH BASIN INLET PROTECTION

8.1 CATCH BASINS AND TRENCH DRAINS WITHIN THE WORK AREA OR THAT WILL RECEIVE RUNOFF FROM THE WORK AREA SHALL BE PROTECTED WITH A SILT SACK AND OR OTHER APPROVED INSTALLATION TO MINIMIZE THE SEDIMENT LOAD TO THE CATCH BASIN

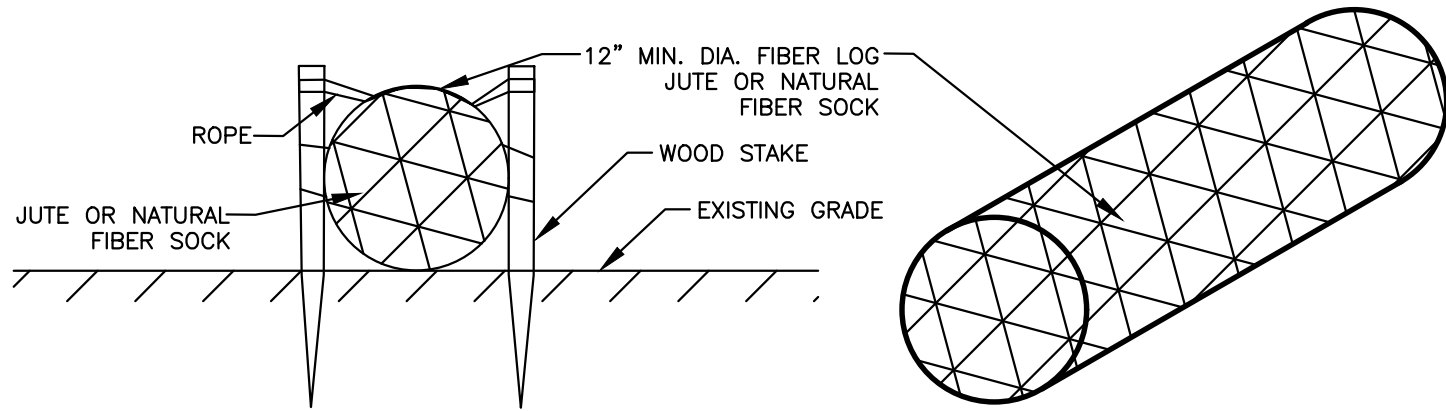
## 9. DUST CONTROL:

9.1 THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES DURING SITE WORK TO MINIMIZE WIND BLOWN DUST FROM EXPOSED SOIL SURFACES. MEASURES INCLUDE BUT ARE NOT LIMITED TO:

- SPRINKLING WATER ON EXPOSED SURFACES
- APPLICATION OF TEMPORARY COVER SUCH AS HYDRO MULCH AND TACKIFIER, STRAW MATTING, JUTE NETTING ETC.



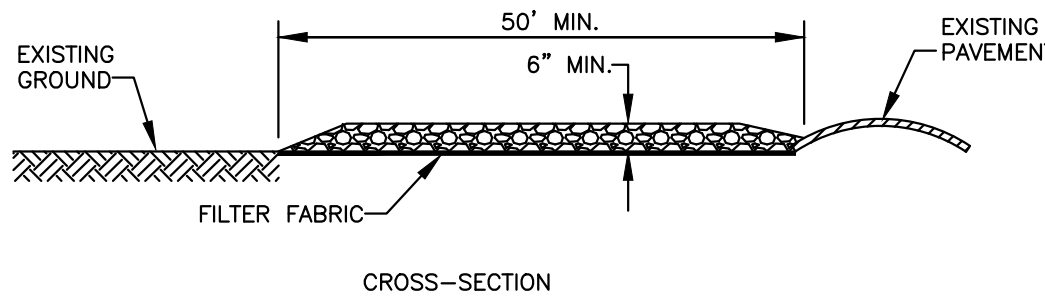
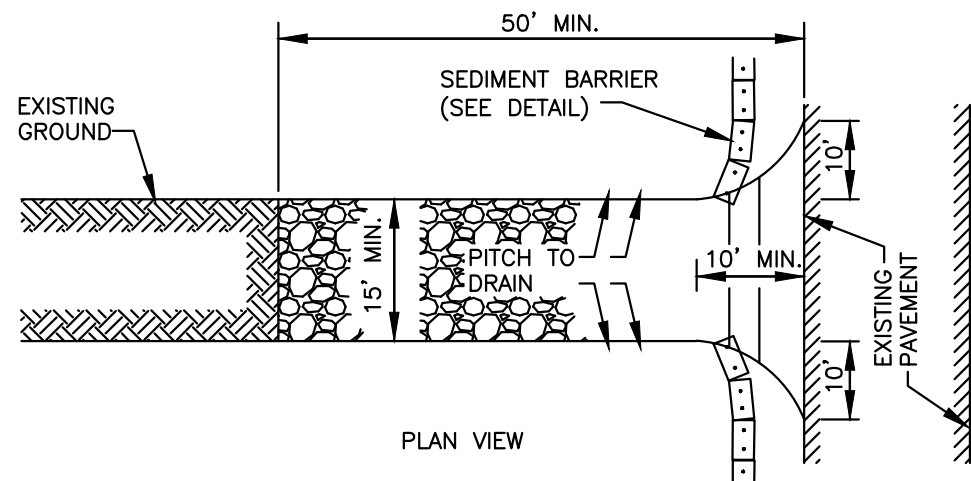
CONSTRUCTION FENCE WITH FIBER LOG DETAIL (NO SCALE)



### INSTALLATION NOTES FOR FIBER LOGS:

1. LAY THE FIBER LOG AT THE UPHILL BASE OF THE SILT FENCE.
2. INSTALL APPROXIMATELY 4-6 WOOD STAKES THROUGH THE TWINE/NETTING ALONG THE FIBER LOG AS NEEDED TO HOLD THE LOG IN PLACE.
3. DRIVE THE STAKE INTO THE GROUND DEEP ENOUGH TO HOLD THE LOG.
4. IN PAVED AREAS, SECURE FIBER LOG WITH CONCRETE BLOCKS OR SAND BAGS.
5. THE FILLING OF THE FIBER LOG MAY BE SHREDED STRAW, COIR, COMPOST OR OTHER APPROVED MATERIAL.
6. FIBER LOG SHALL BE 12 INCHES (MIN) IN DIAMETER UNLESS OTHERWISE NOTED ON THE PLANS.

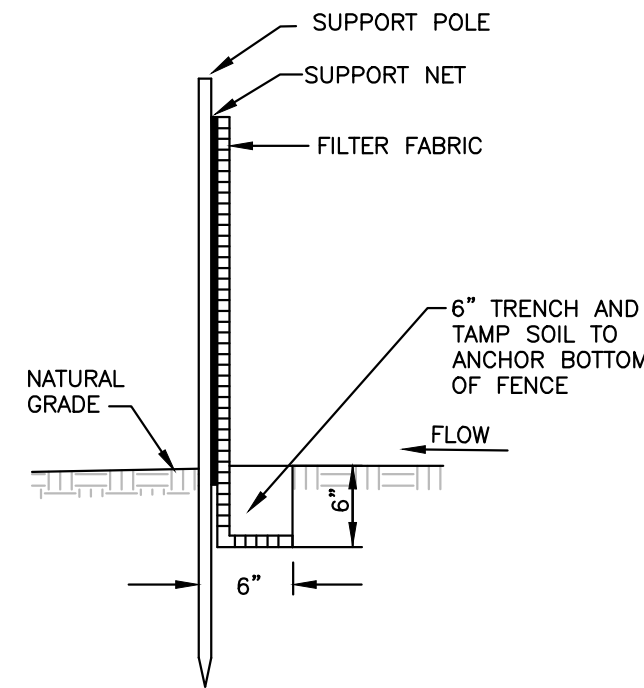
SEDIMENT BARRIER (FIBER LOG) (NO SCALE)



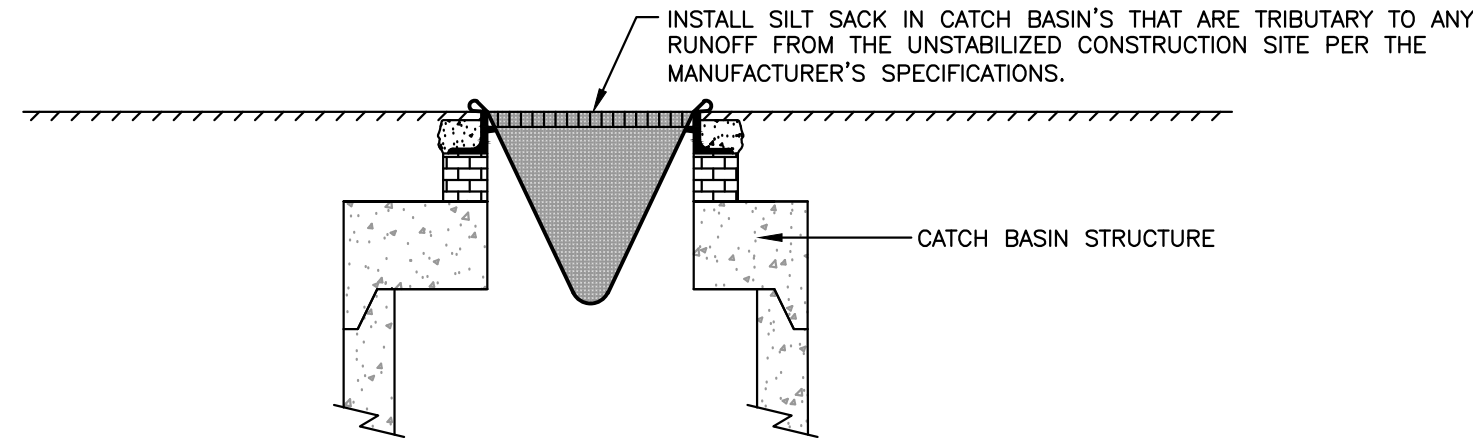
### CONSTRUCTION SPECIFICATIONS:

1. STONE SIZE - USE 1 1/2" TO 3 1/2" WASHED, ANGULAR STONE.
2. THICKNESS - NOT LESS THAN SIX (6) INCHES.
3. WIDTH - FIFTEEN (15) FEET MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
4. FILTER FABRIC - SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. USE MIRAFI HP-370 OR EQUAL.
5. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH SHALL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF WAY MUST BE REMOVED IMMEDIATELY.
6. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED.

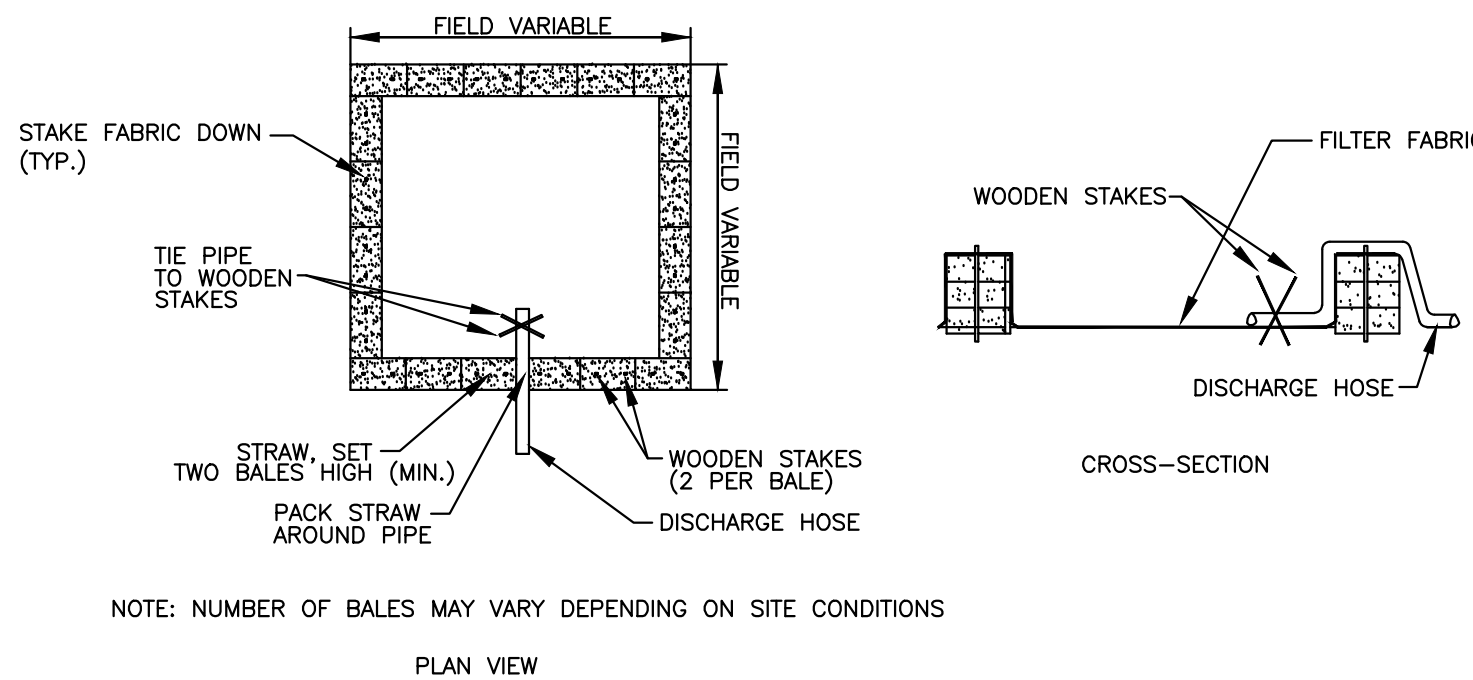
STONE TRACKING PAD (CONSTRUCTION ENTRANCE) (NO SCALE)



SILT FENCE INSTALLATION (NO SCALE)



SILT SACK DETAIL (NO SCALE)



NOTE: NUMBER OF BALES MAY VARY DEPENDING ON SITE CONDITIONS

### PLAN VIEW

### DEWATERING OF EXCAVATIONS NOTES:

1. DISCHARGE FROM DEWATERING PUMPS OR TEMPORARY TRENCH OR EXCAVATION DRAINS SHALL NOT DISCHARGE DIRECTLY TO WETLAND RESOURCE AREAS, ON-SITE SUBSURFACE SEWAGE DISPOSAL SYSTEM, OR EXISTING/PROPOSED DRAINAGE SYSTEMS. THE DISCHARGES SHALL BE DIRECTED INTO A CONSTRUCTED SEDIMENT BASIN OR A STRAW BALE SETTLING BASIN.

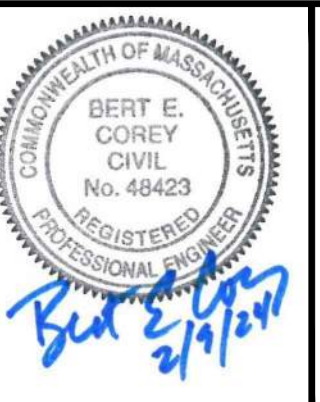
STRAW BALE SETTLING BASIN (NO SCALE)

PARCEL ID:

MAP 7, LOT 0, BLOCK 49

ISSUED FOR:

COMPREHENSIVE  
PERMIT APPLICATION



NO.	APP	DATE	DESCRIPTION
1	BEC	2/9/24	DESIGN DEVELOPMENT

DATE: JANUARY 23, 2024

SCALE: AS NOTED

DESIGN:	DRAFTED:	CHECKED:
KMR	KMR/JAL	JAL/BEC

PROJECT TITLE:

WASHINGTON  
STREET  
SHERBORN  
HOMES

0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770

SHEET TITLE:

EROSION AND  
SEDIMENT CONTROL  
NOTES AND DETAILS

SHEET:

7 OF 12

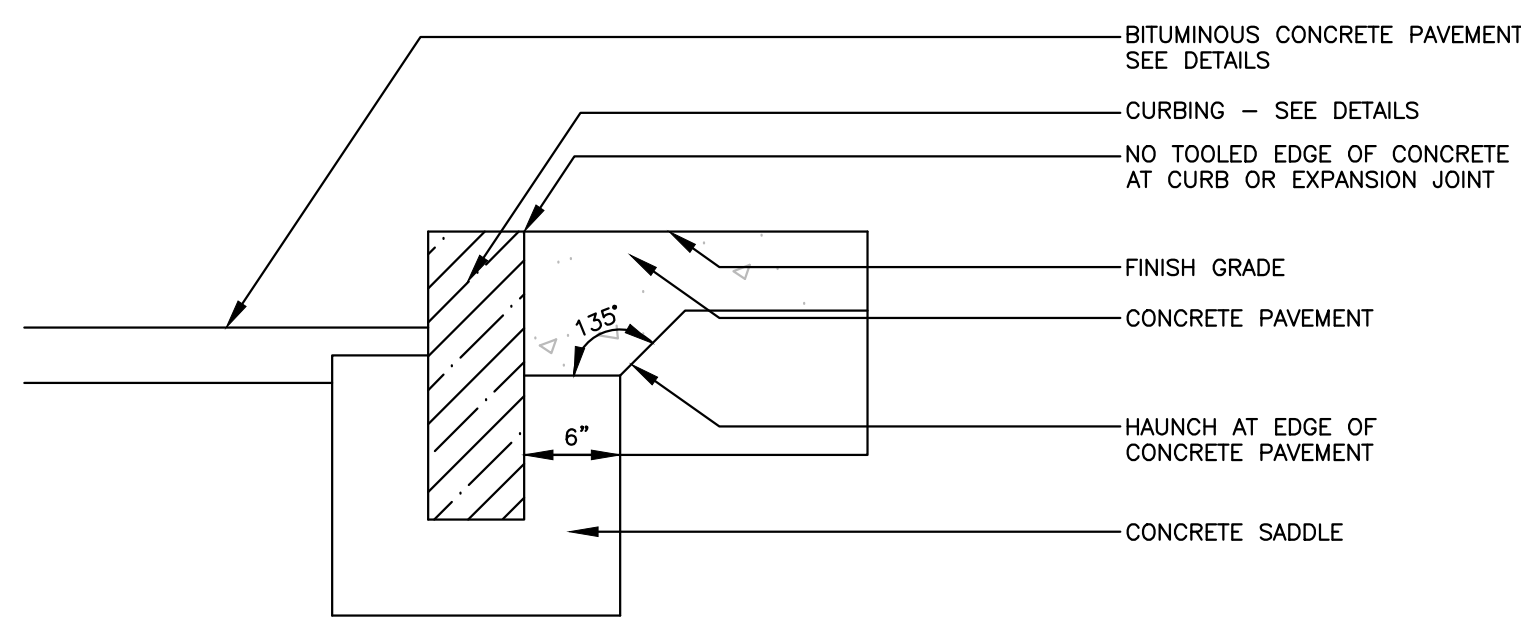
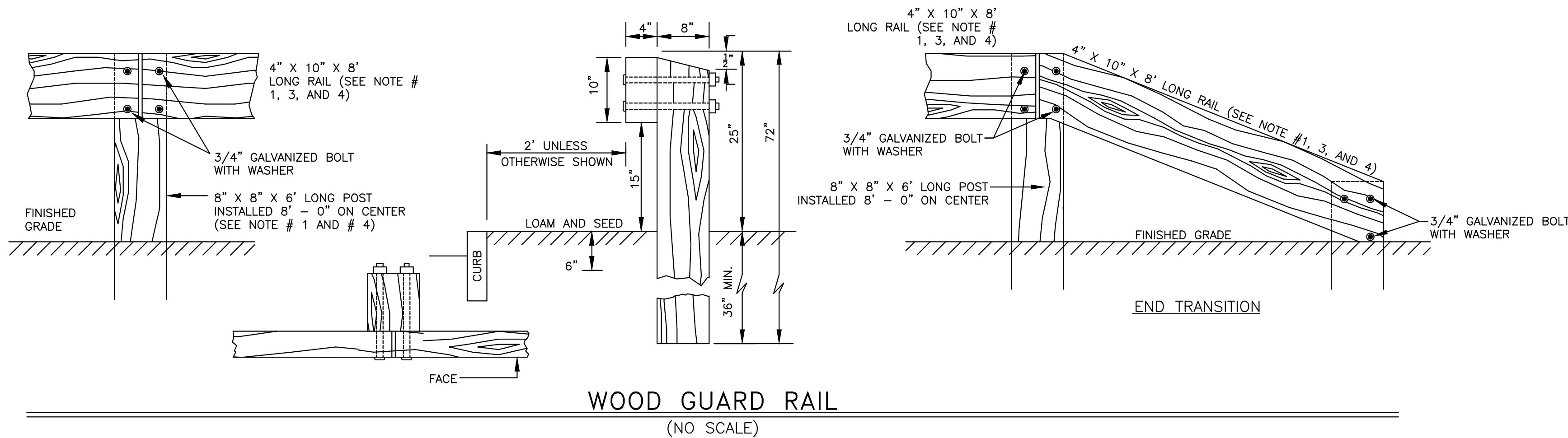
PROJECT NO.:

F-25902

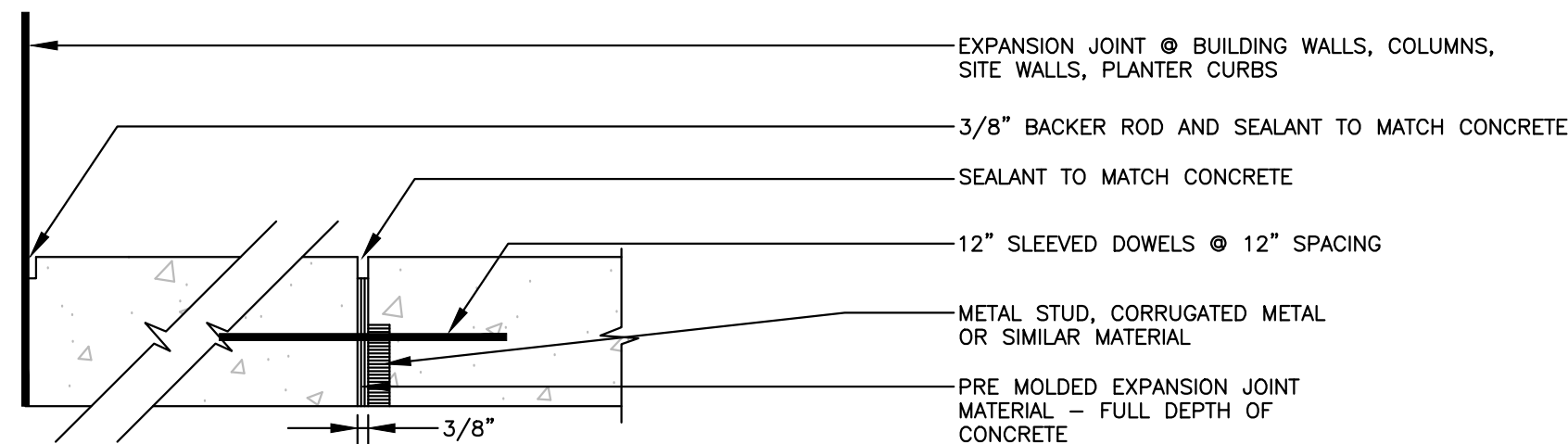
C-7



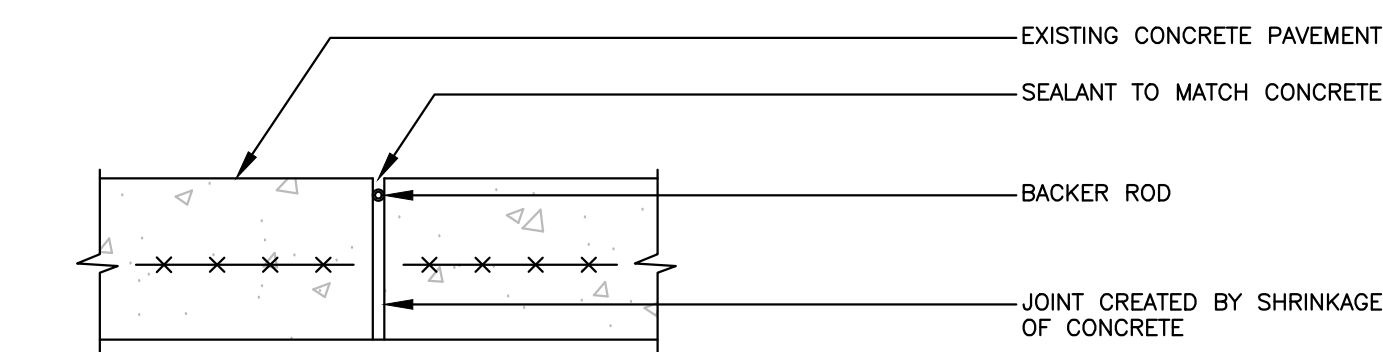
- NOTES:
1. POSTS SHALL BE INSTALLED 36" MIN. DOWN FROM FINISHED GRADE.
  2. REFLECTORS TO BE INSTALLED MIDWAY BETWEEN POST LOCATIONS.
  3. ALL TIMBER MATERIAL SHALL BE SELECT STRUCTURAL DOUGLAS FIR OR SOUTHERN YELLOW PINE, PRESSURE TREATED WITH ALKALINE COPPER QUATERNARY (ACQ).
  4. ENDS OF GUARD RAILS HAVE A TRANSITION TO FINISHED GROUND LEVEL.



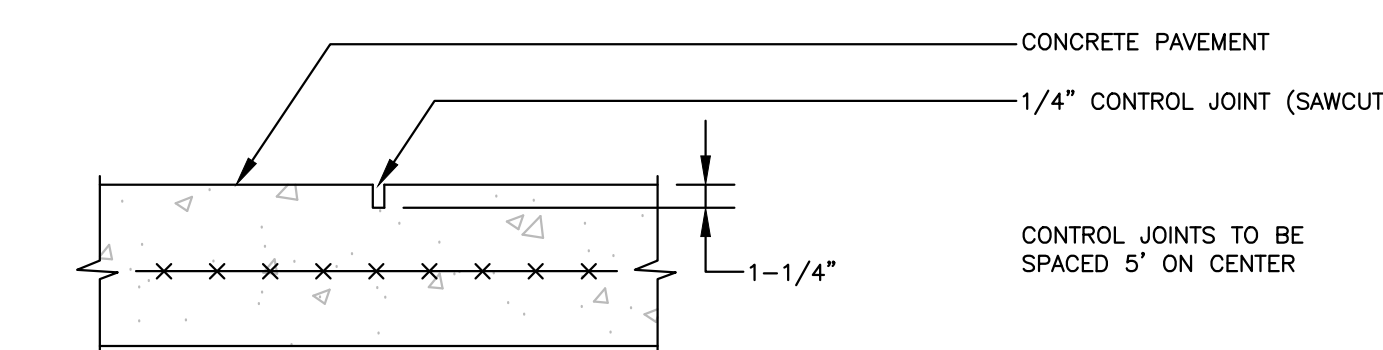
E CURB EDGE



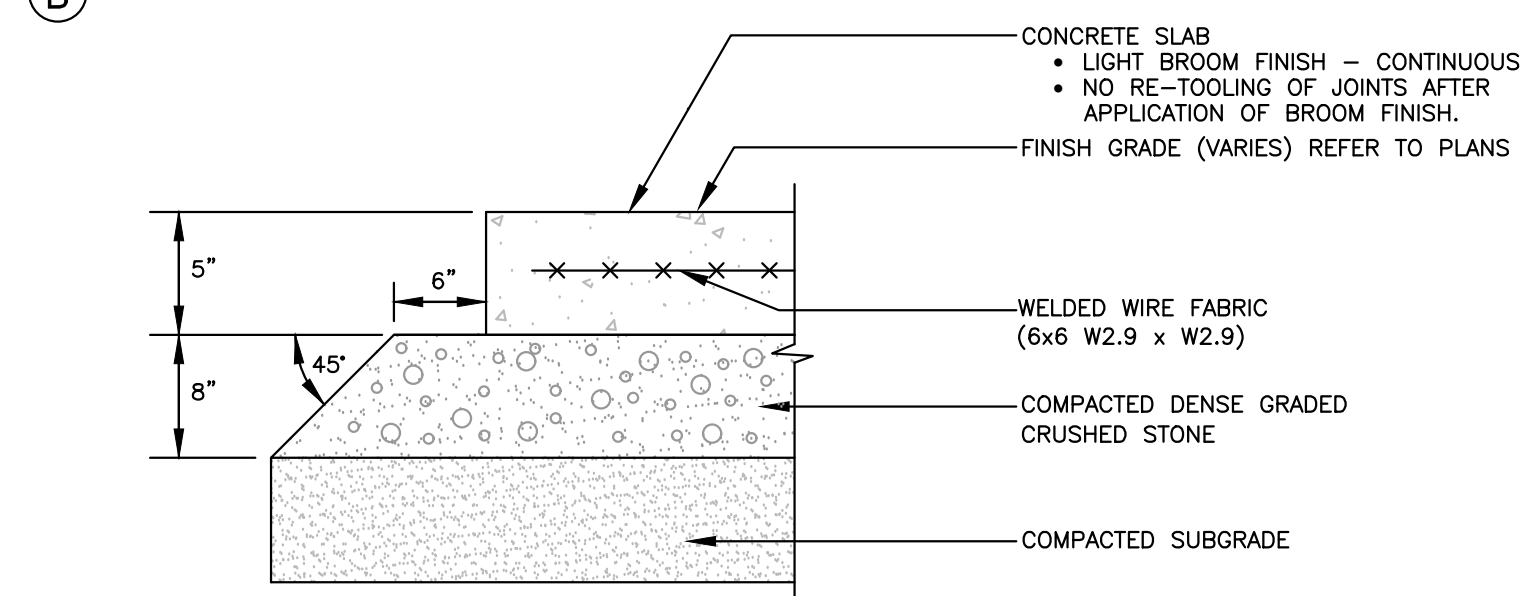
D EXPANSION JOINT



C CONSTRUCTION JOINT

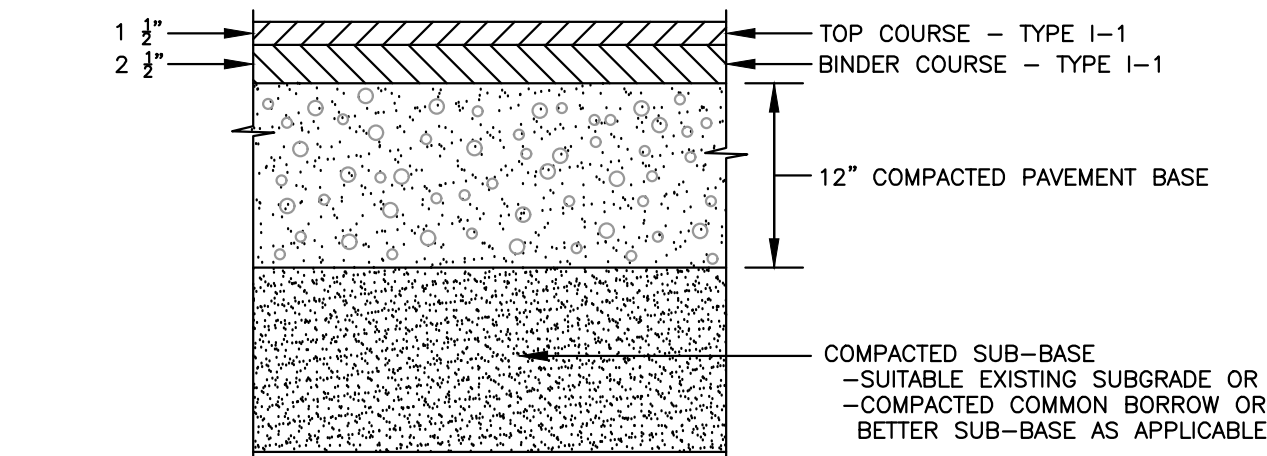


B CONTROL JOINT



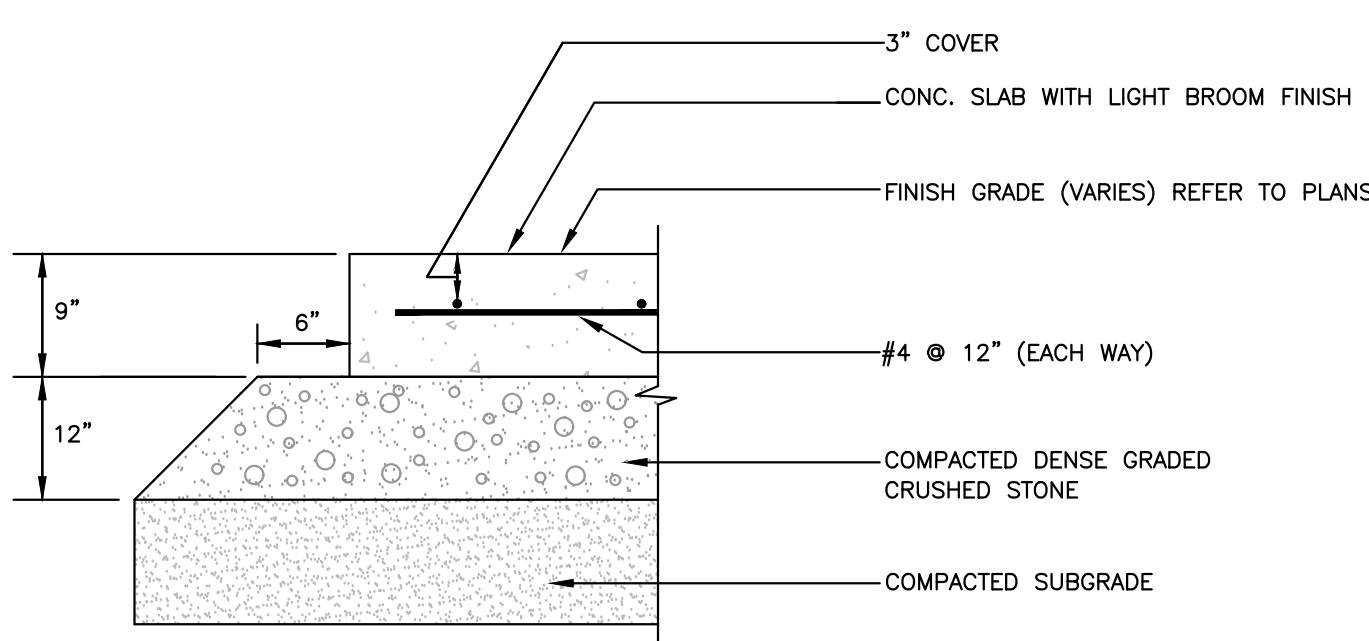
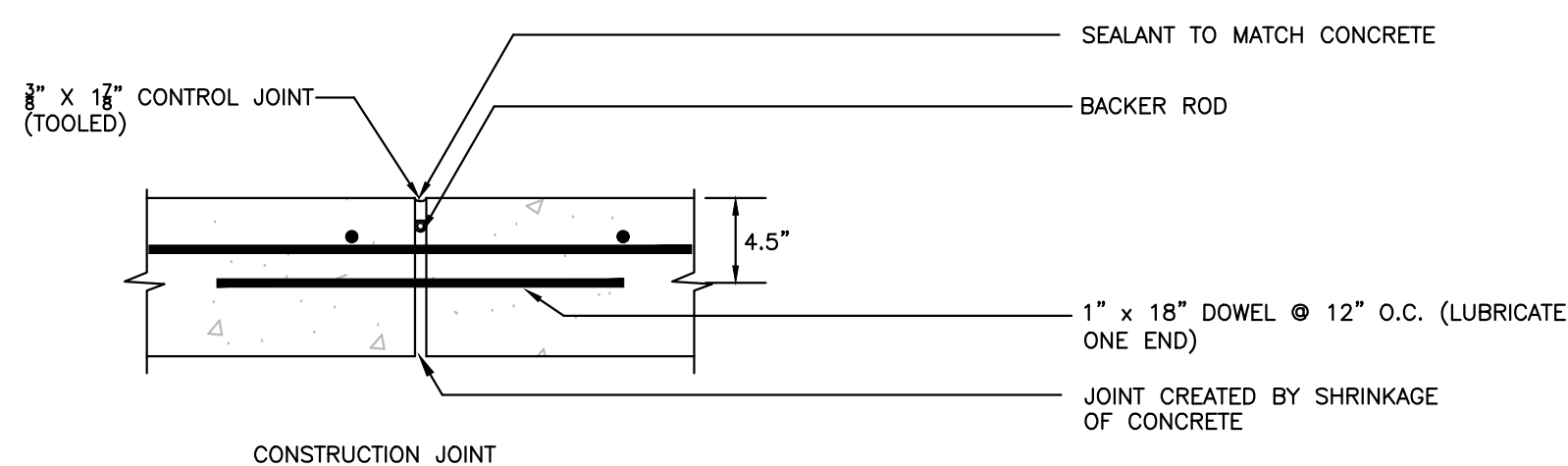
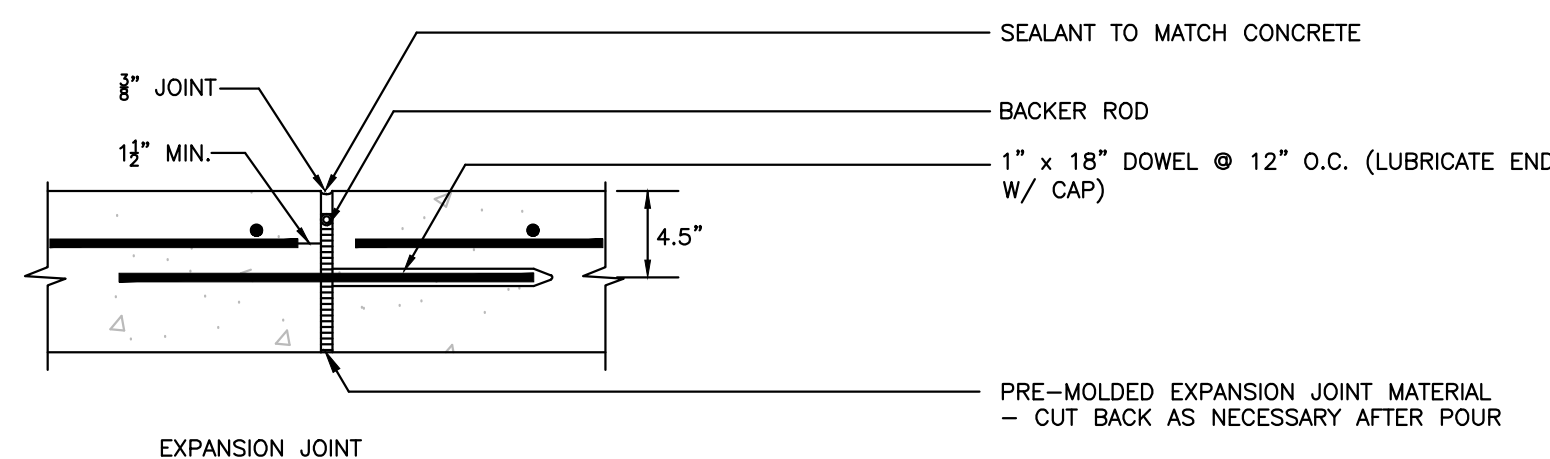
A SECTION

CONCRETE SIDEWALK DETAILS  
(NO SCALE)



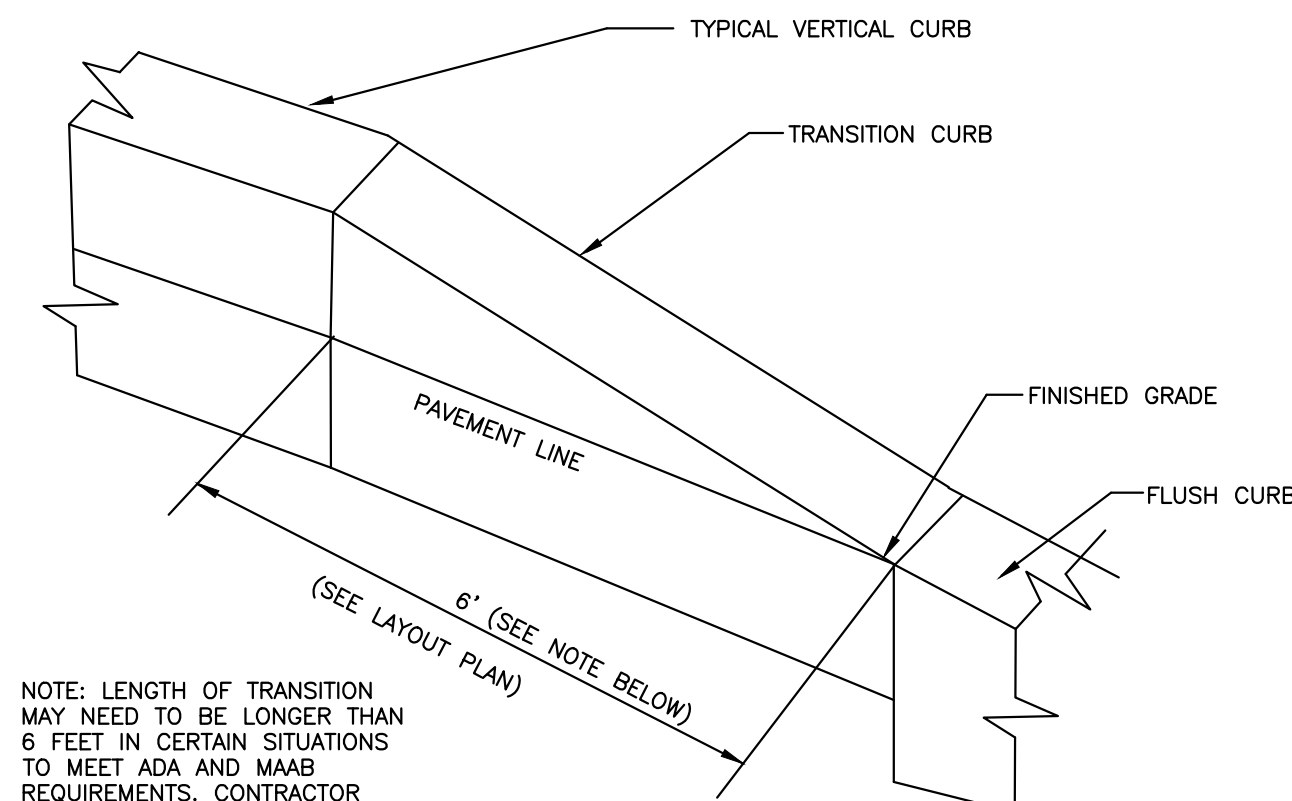
- NOTES:
1. COMPACTED PAVEMENT BASE TO CONFORM TO MASS. HIGHWAY DEPT. SPEC. M 2.01.7.
  2. COMPACTED COMMON BORROW. NO STONES LARGER THAN 6 INCHES.
  3. BITUMINOUS CONCRETE SHALL BE CLASS 1, TYPE 1-1.
  4. AT AREAS OF LEDGE/BEDROCK, REMOVE LEDGE TO A DEPTH OF 18 INCHES MINIMUM BELOW PAVEMENT COURSE.
  5. COMPACTED PAVEMENT BASE TO EXTEND 12 INCHES MINIMUM BEYOND THE EDGE OF PAVEMENT.

BITUMINOUS CONCRETE PAVEMENT  
(NO SCALE)

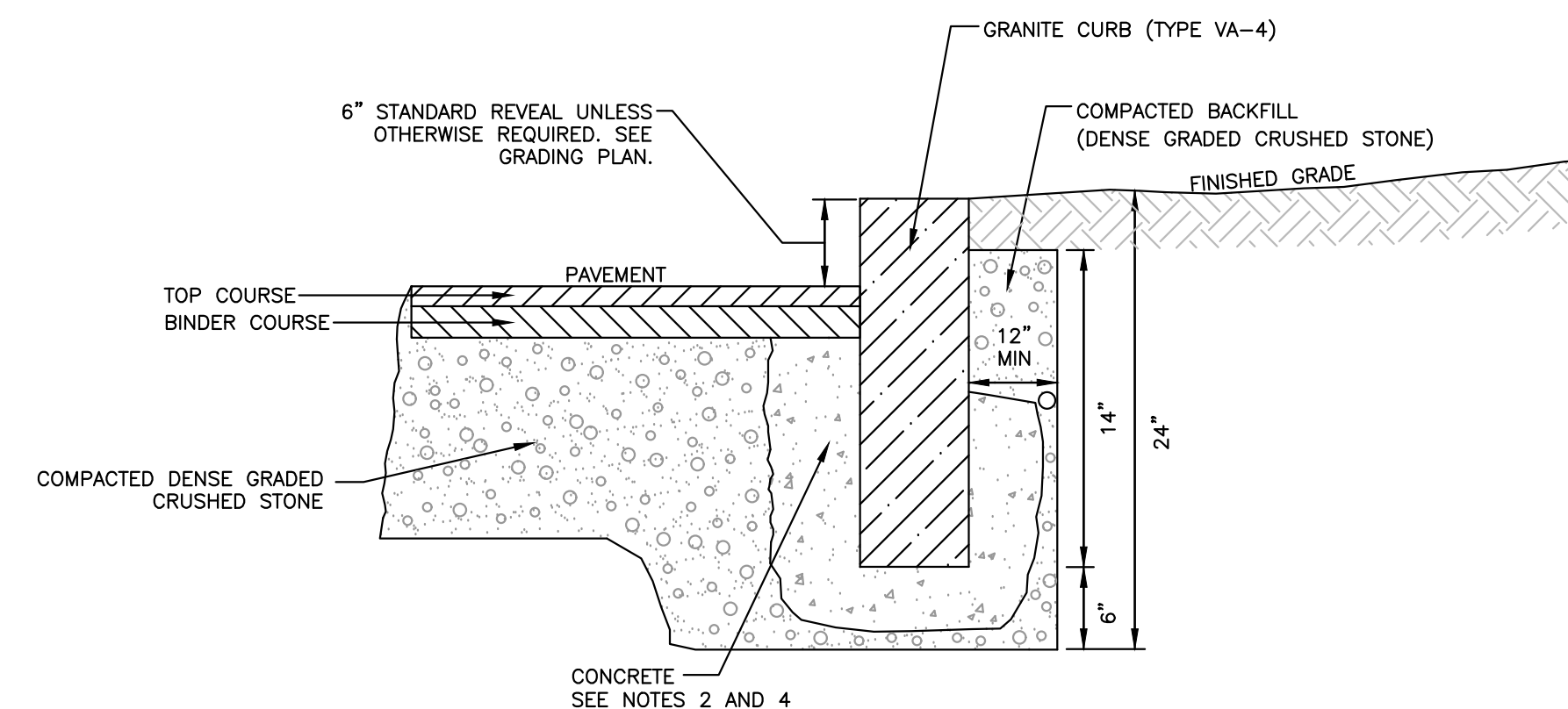
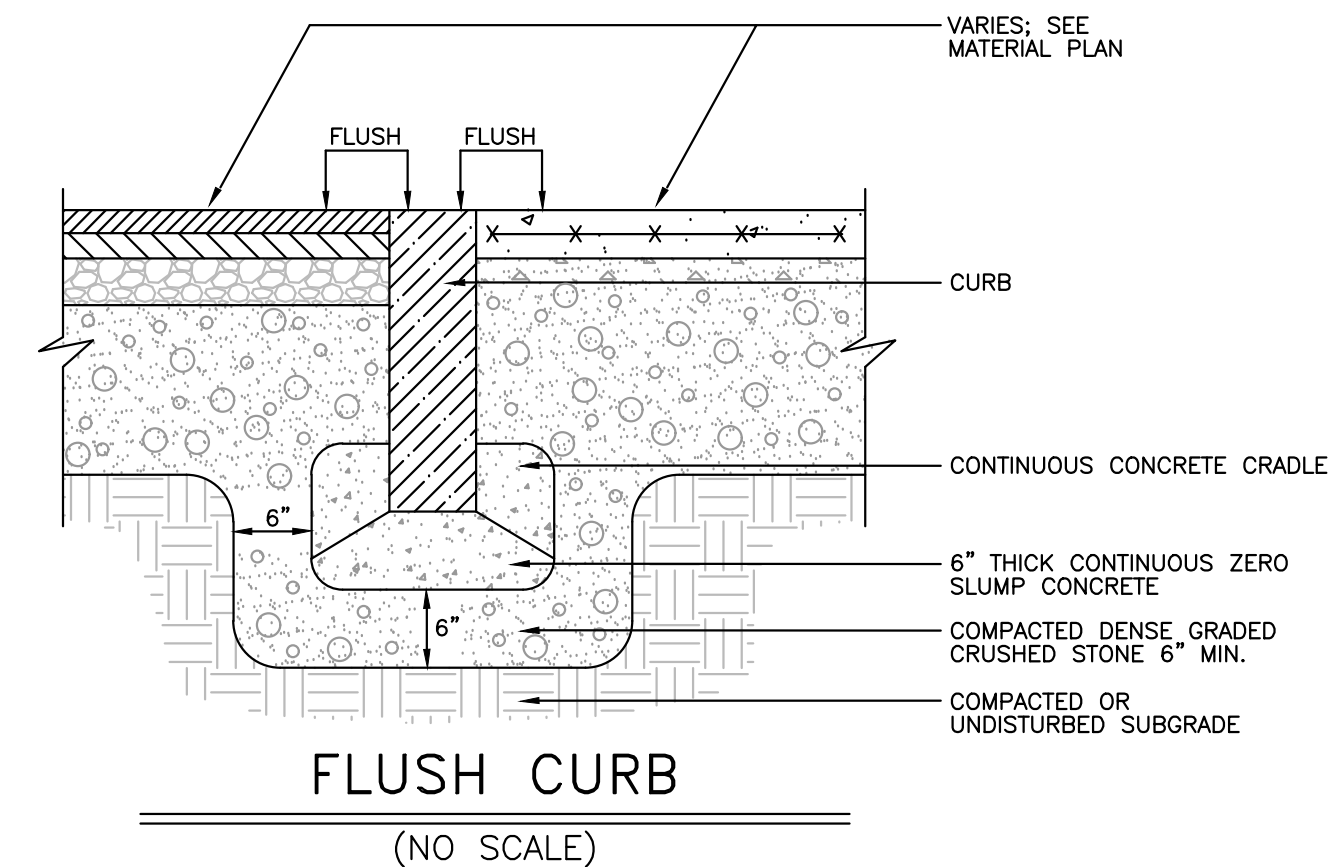


- NOTES:
1. EXPANSION JOINTS 20'-0" O.C. UNLESS SHOWN OTHERWISE.
  2. CONTROL JOINTS 5'-0" O.C. UNLESS SHOWN OTHERWISE.
  3. TOOLED EDGES AND JOINTS, ALL PANELS.
  4. WHERE SAWCUT JOINT IS REQUIRED, DEPTH TO BE 1 1/2".
  5. WHERE EXPANSION JOINT ABUTS WALL OR OTHER VERTICAL SURFACE, DELETE EXPANSION SLEEVE AND DOWEL.
  6. ALL TOOL MARKS ARE TO BE SMOOTHED OUT BY A LIGHT BROOM FINISH AFTER APPLICATION.
  7. VEHICULAR CONCRETE SHALL BE 4000 PSI CONCRETE, 7% AIR ENTRAINMENT.

VEHICULAR CONCRETE PAVEMENT FOR THE DUMPSTER PAD  
(NO SCALE)



TRANSITIONAL CURB  
(NO SCALE)



- NOTES:
1. CURB INSTALLATION ALONG EXISTING ROAD PAVEMENT SHALL BE SAW CUT AND PATCHED WITH A 2 1/2" BITUMINOUS GRADE BINDER COURSE, AND TOPPED WITH A 1-1/2" WEAR COURSE OF TYPE "1". THE CUT JOINT SHALL BE THOROUGHLY SEALED WITH ASPHALT EMULSION AND SAND. TOP OF CURB SHALL BE 6" ABOVE GUTTER GRADE.
  2. AT ALL LOCATIONS WHERE GRANITE CURBING IS PROPOSED, INCLUDING REPLACEMENT, THE CURB IS TO BE SET IN 6" THICK CONTINUOUS ZERO SLUMP CONCRETE.
  3. GRANITE CURB INSTALLATION TO BE PER MHD SECTION 501 AND THE LATEST MUNICIPAL STANDARDS.
  4. CONCRETE TO BE AIR ENTRAINED CLASS D CONFORMING TO MHD M4.02.00.
  5. GROUTING OF CURB JOINTS TO BE NON-SHRINK GROUTING CONFORMING TO ASTM C-827.

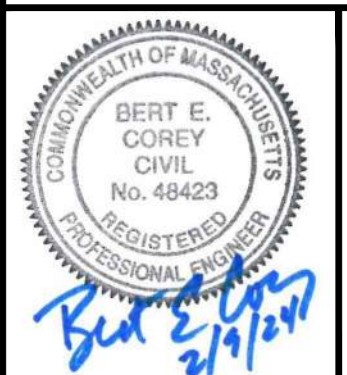
GRANITE CURB  
(NO SCALE)

PARCEL ID:

MAP 7, LOT 0, BLOCK 49

ISSUED FOR:

COMPREHENSIVE  
PERMIT APPLICATION



NO.	APP	DATE	DESCRIPTION
1	BEC	2/9/24	DESIGN DEVELOPMENT

DATE: JANUARY 23, 2024

SCALE: AS NOTED

DESIGN:	DRAFTED:	CHECKED:
KMR	KMR/JAL	JAL/BEC

PROJECT TITLE:

WASHINGTON  
STREET  
SHERBORN  
HOMES

0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770

SHEET TITLE:

SITE DETAILS - 01

SHEET:  
8 OF 12

PROJECT NO.:  
F-25902

C-8



PARCEL ID:

**MAP 7, LOT 0, BLOCK 49**

ISSUED FOR:

**COMPREHENSIVE  
PERMIT APPLICATION**



1	BEC	2/9/24	DESIGN DEVELOPMENT
NO.	APP	DATE	DESCRIPTION

DATE: **JANUARY 23, 2024**

SCALE: **AS NOTED**

DESIGN: <b>KMR</b>	DRAFTED: <b>KMR/JAL</b>	CHECKED: <b>JAL/BEC</b>
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PROJECT TITLE:

**WASHINGTON  
STREET  
SHERBORN  
HOMES**

**0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770**

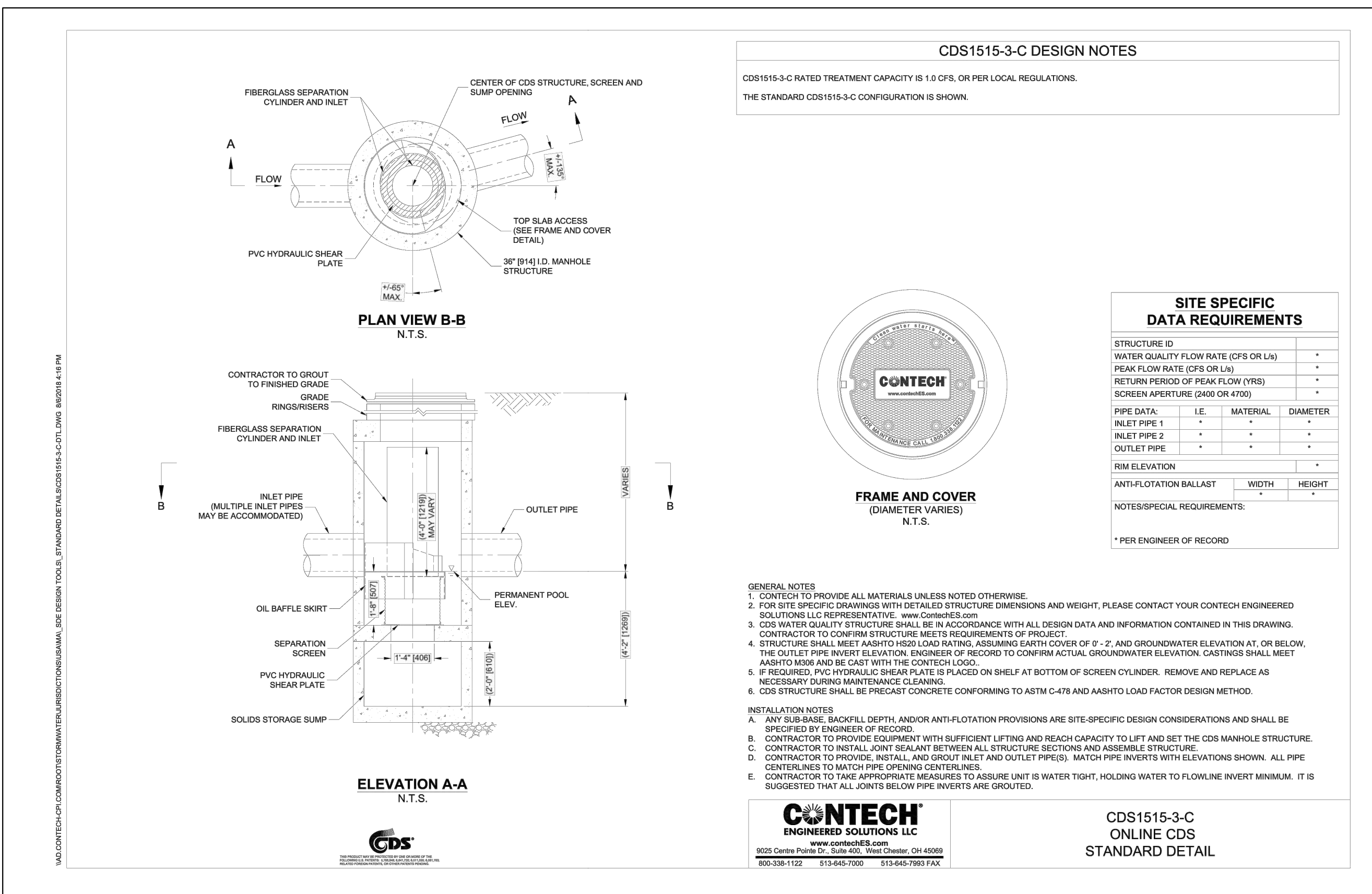
SHEET TITLE:

**SITE DETAILS - 02**

SHEET:  
**9 OF 12**

PROJECT NO.:  
**F-25902**

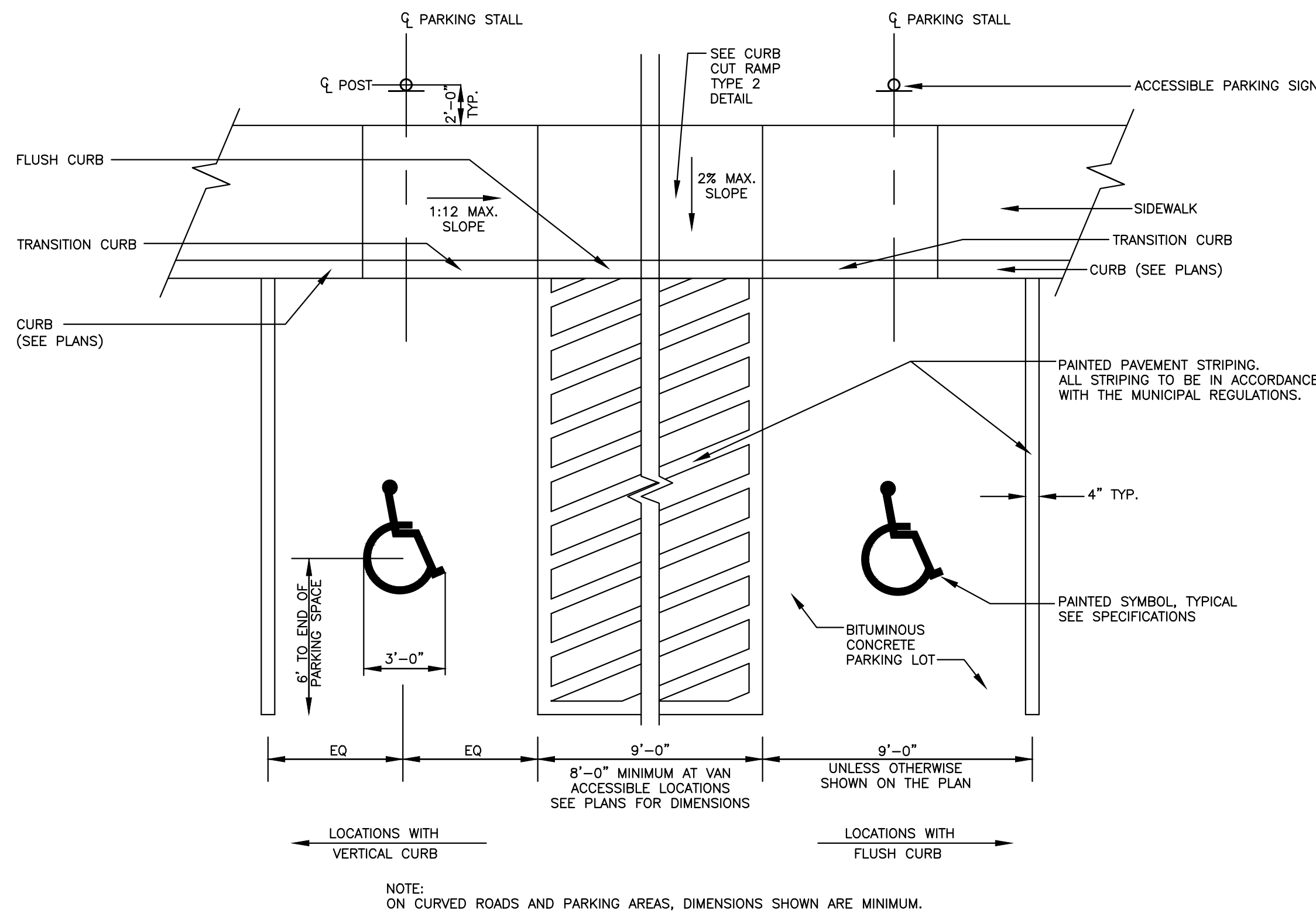
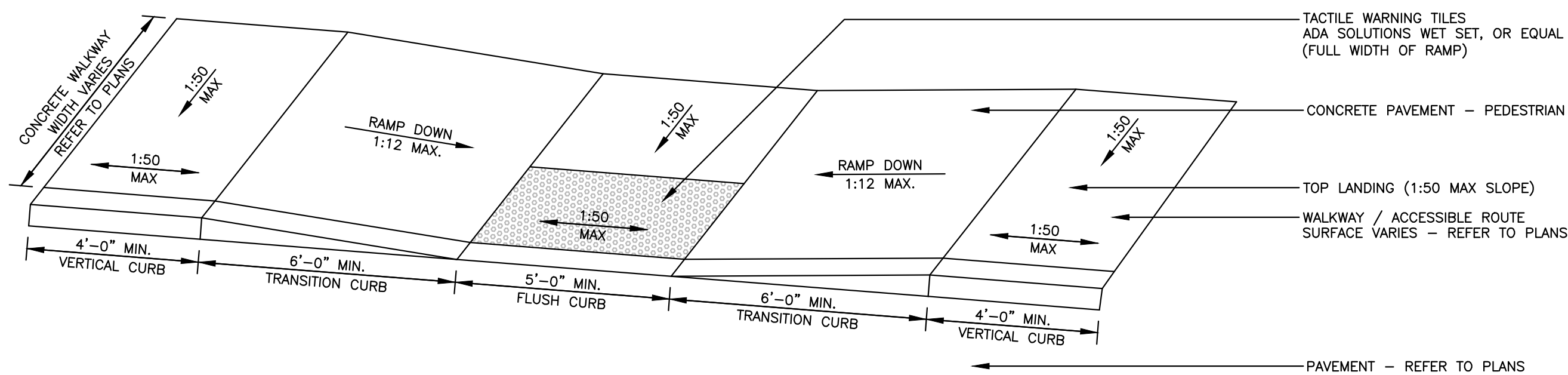
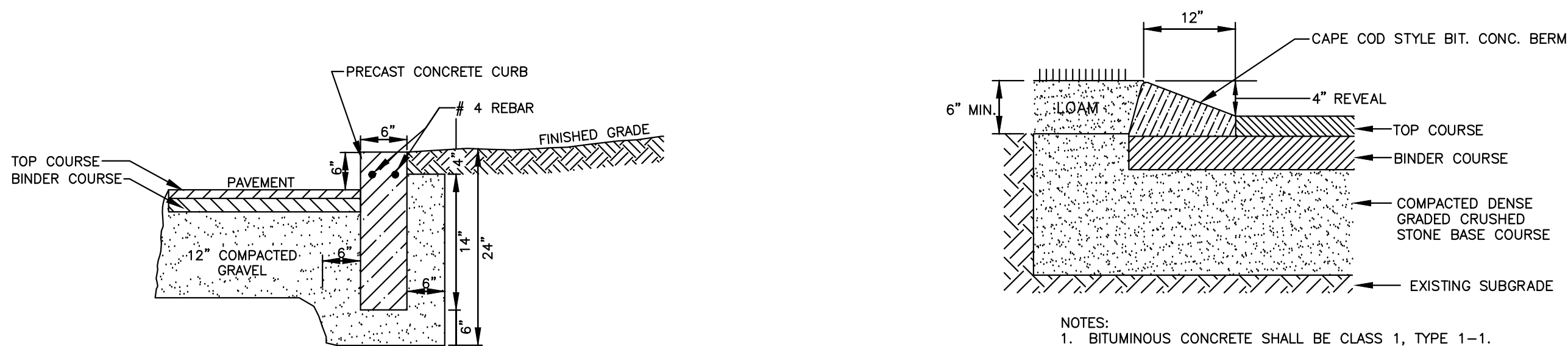
**C-9**



THE DESIGN OF STU #1, STU #2, STU #3, STU #4 AND STU #5 IS BASED ON THE USE OF A CDS1515-3-C ONLINE CDS UNIT BY CONTECH ENGINEERED SOLUTIONS LLC IN THIS DETAIL. IF A DIFFERENT UNIT IS TO BE PROPOSED BY THE CONTRACTOR, THEN SAID CONTRACTOR SHALL PROVIDE SHOP DRAWINGS OF THE UNIT PROPOSED WITH WATER QUALITY SIZING CALCULATIONS SHOWING CONFORMANCE WITH THE DESIGN REQUIREMENTS. THE SHOP DRAWINGS AND CALCULATIONS SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR REVIEW AND APPROVAL.

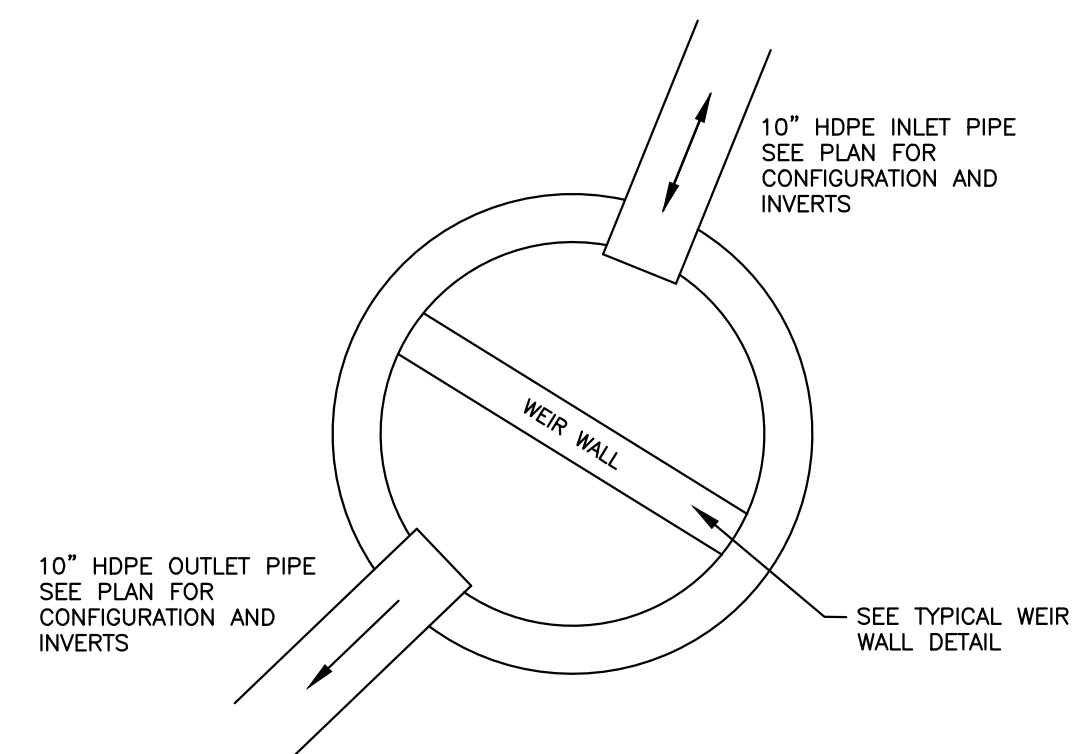
**STORMWATER TREATMENT UNIT (STU #1, STU #2, STU #3, STU #4, STU #5) DETAIL**

(NO SCALE)

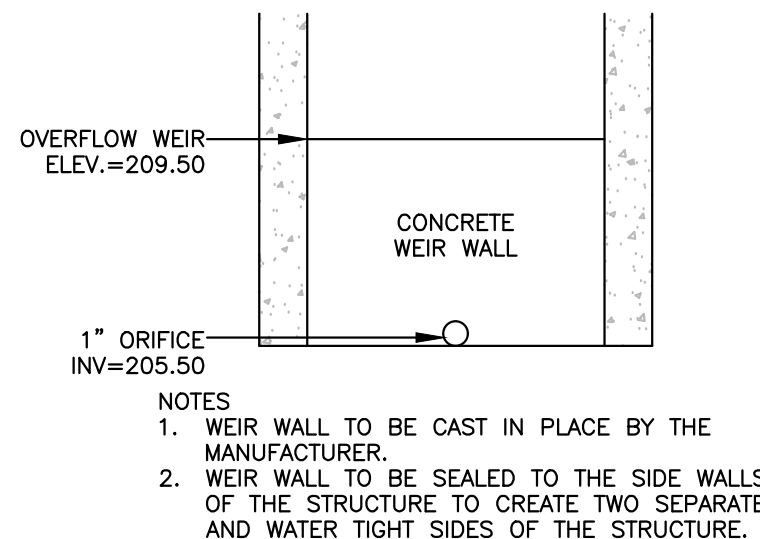


NOTE:  
ON CURVED ROADS AND PARKING AREAS, DIMENSIONS SHOWN ARE MINIMUM.

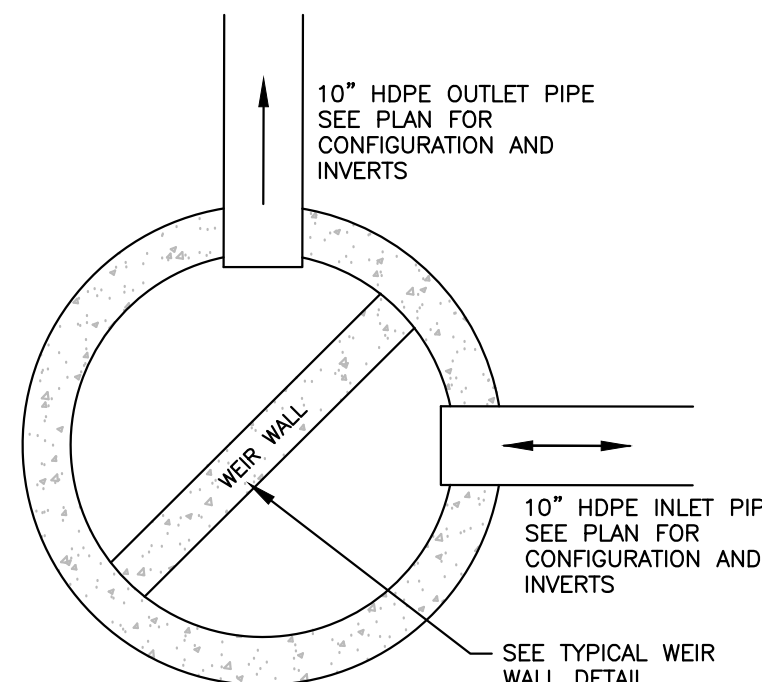




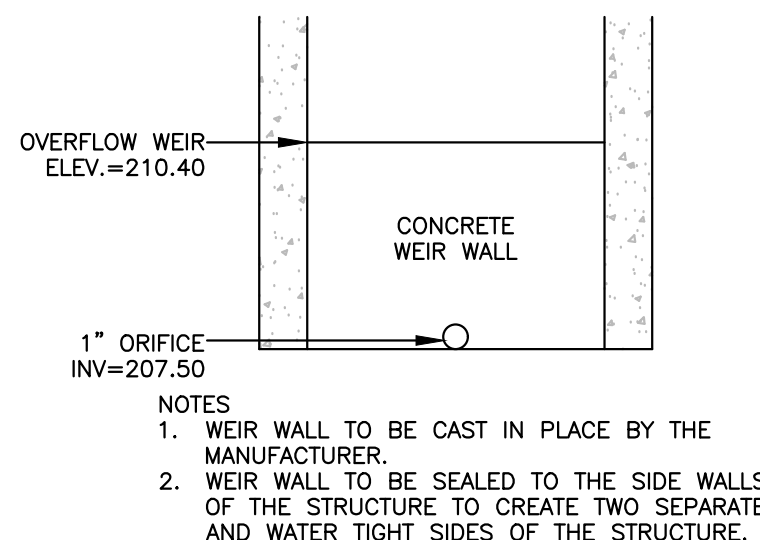
OCS #2  
TYPICAL PLAN VIEW



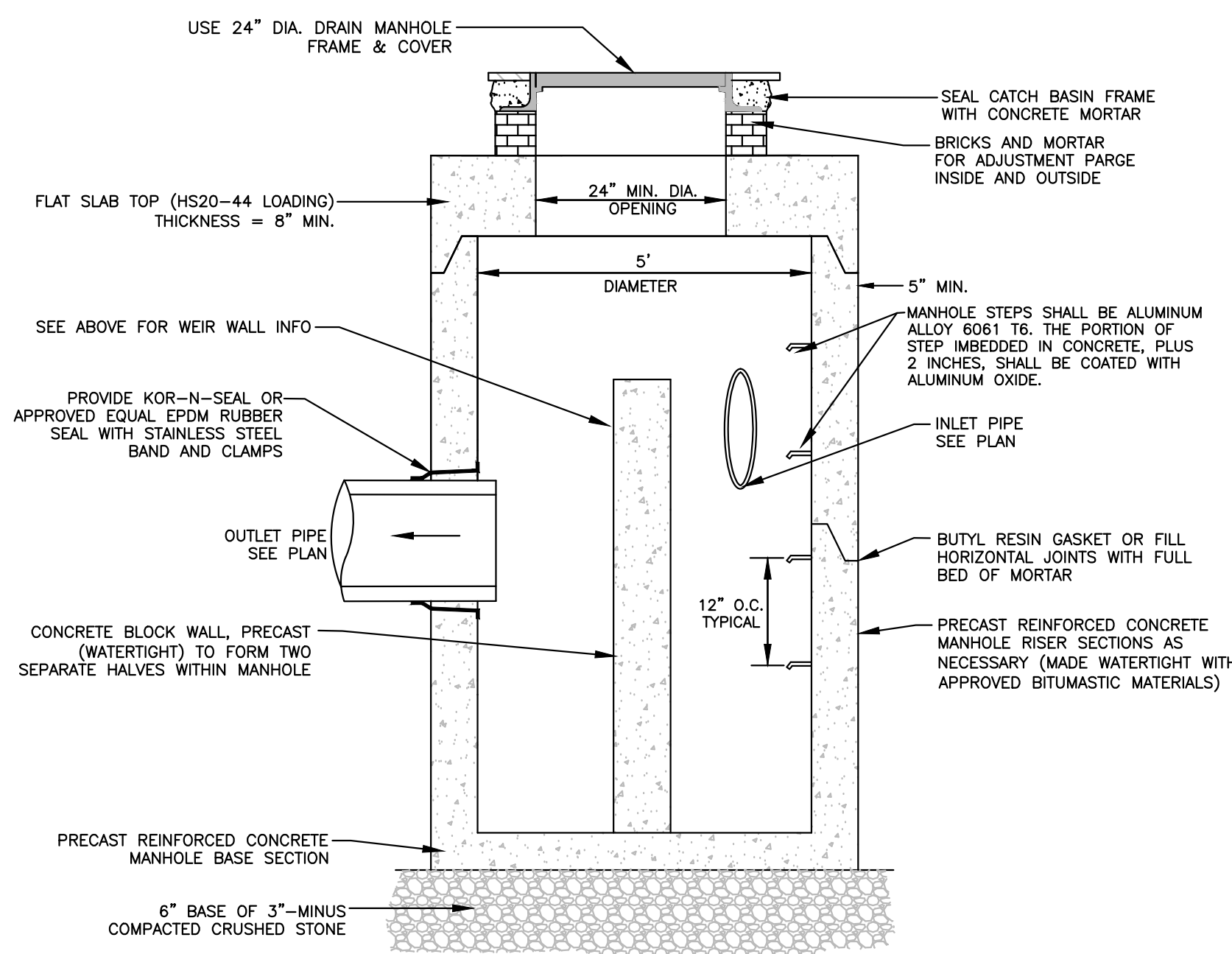
OCS #2  
TYPICAL WEIR WALL DETAIL



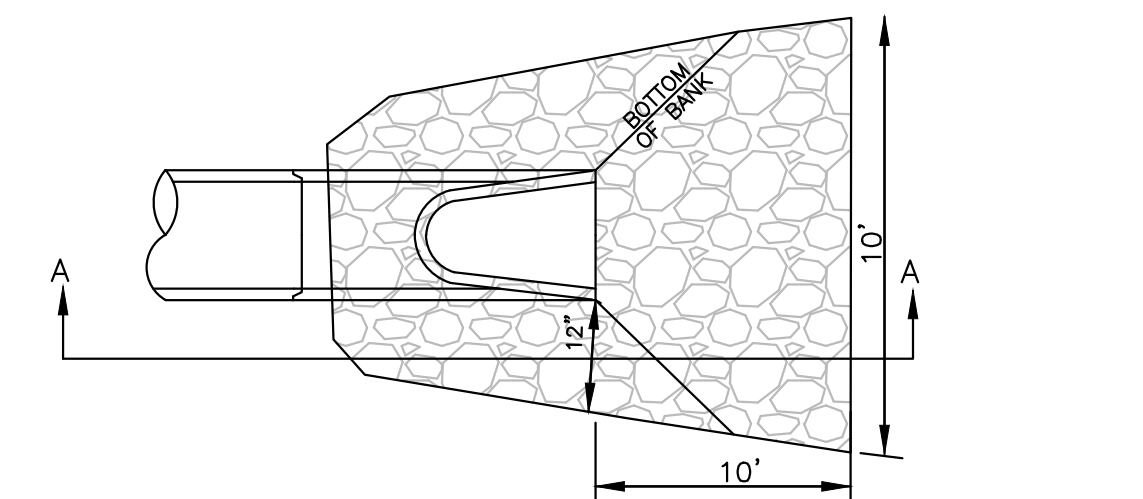
OCS #1  
TYPICAL PLAN VIEW



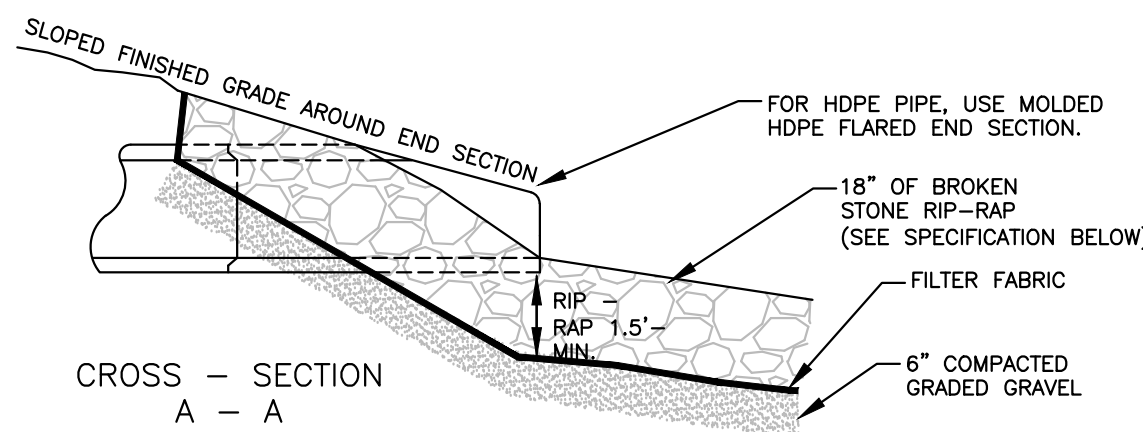
OCS #1  
TYPICAL WEIR WALL DETAIL



OUTLET CONTROL STRUCTURE #1 & #2  
(NO SCALE)



PLAN VIEW

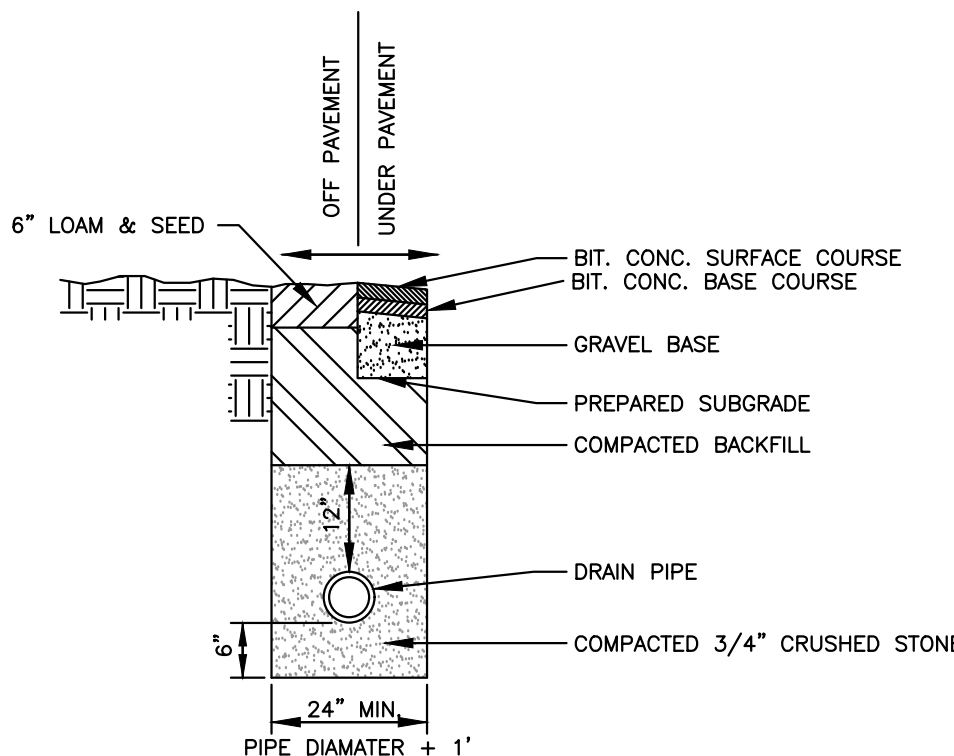


NOTES:

1. RIP-RAP SHALL CONSIST OF EVENLY GRADED 6" TO 12" ANGULAR BROKEN STONE, (AVG. STONE SIZE = 9") WITH A THICKNESS OF 18 INCHES. NOT MORE THAN 15 PERCENT OF THE STONE MAY BE SCATTERED SPALLS AND STONES LESS THAN 4" IN SIZE.
2. SIZE DESIGNATION REFERS TO MEAN SPHERICAL DIAMETER.
3. LENGTH OF RIP-RAP TO BE PER THIS DETAIL.

PIPE END SECTION  
WITH RIP - RAP APRON

(NO SCALE)

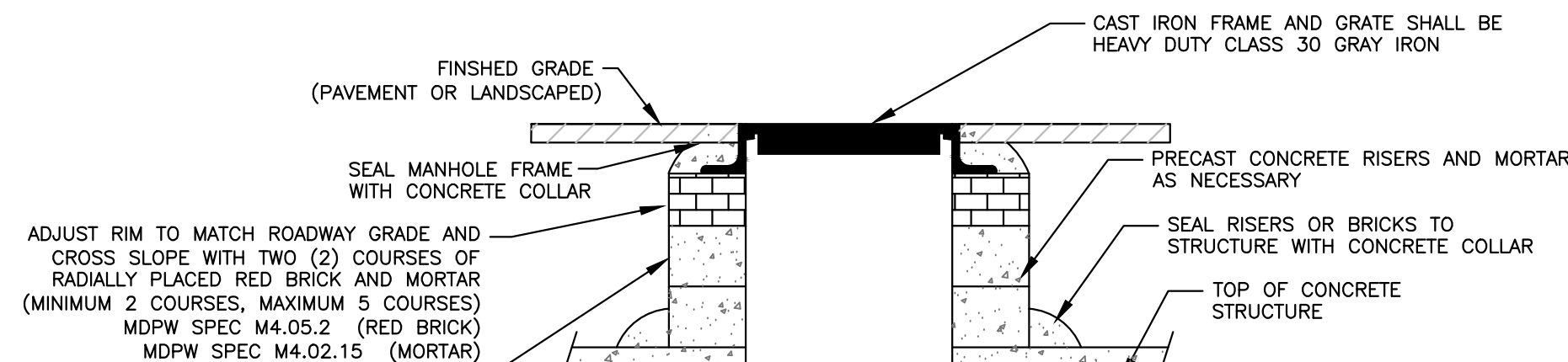


NOTES:

1. TRENCH BACKFILL SHALL CONFORM TO THE FOLLOWING SPECIFICATIONS AS CONTAINED IN MASSACHUSETTS HIGHWAY DEPARTMENT, STANDARDS AND SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, 1988.

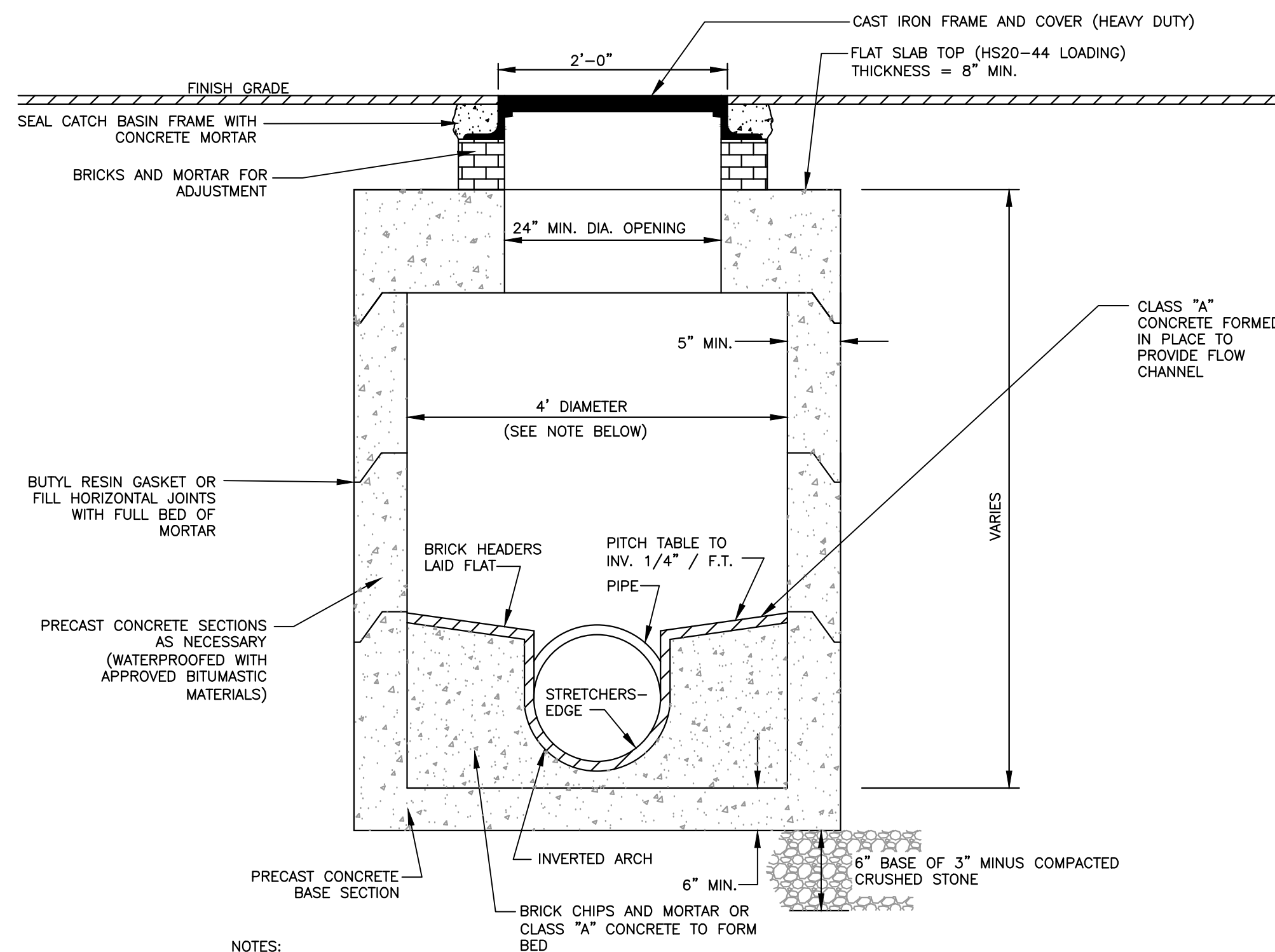
TYPICAL DRAIN  
PIPE BEDDING

(NO SCALE)



FRAME & COVER/GRATE  
INSTALLATION DETAIL

(NO SCALE)

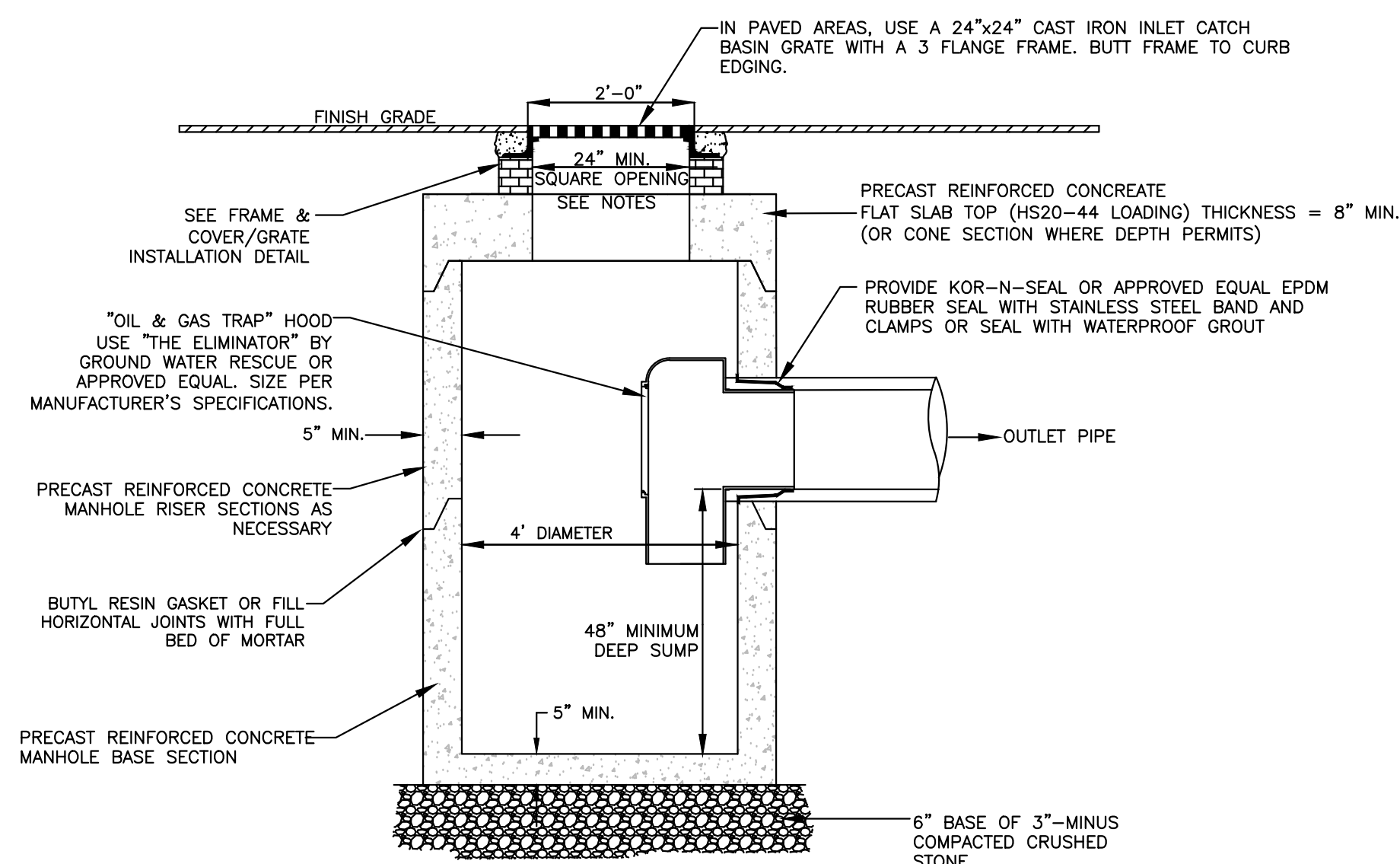


NOTES:

1. USE 5 FT. DIAMETER (OR LARGER WHERE NECESSARY) WHEN TOTAL INSIDE DEPTH EXCEEDS 12 FT. OR WHERE PIPE ARRANGEMENT WILL DAMAGE THE INTEGRITY OF THE MANHOLE SECTIONS.
2. MANHOLE CONNECTION TO PIPE SHALL BE BY MEANS OF A FLEXIBLE RUBBER SLEEVE OR MORTAR. PROJECT PIPE 1"-3" INTO MANHOLE.

DRAIN MANHOLE DETAIL

(NO SCALE)



DEEP SUMP CATCH BASIN

(NO SCALE)

PARCEL ID:

MAP 7, LOT 0, BLOCK 49

ISSUED FOR:

COMPREHENSIVE  
PERMIT APPLICATION



1	BEC	2/9/24	DESIGN DEVELOPMENT
NO.	APP	DATE	DESCRIPTION

DATE: JANUARY 23, 2024

SCALE: AS NOTED

DESIGN:	DRAFTED:	CHECKED:
KMR	KMR/JAL	JAL/BEC

PROJECT TITLE:

WASHINGTON  
STREET  
SHERBORN  
HOMES

0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770

SHEET TITLE:

SITE DETAILS - 03

SHEET:  
10 OF 12

PROJECT NO.:  
F-25902

C-10



PARCEL ID:

MAP 7, LOT 0, BLOCK 49

ISSUED FOR:

COMPREHENSIVE  
PERMIT APPLICATION



NO.	APP	DATE	DESCRIPTION
1	BEC	2/9/24	DESIGN DEVELOPMENT

DATE: JANUARY 23, 2024

SCALE: AS NOTED

DESIGN: KMR	DRAFTED: KMR/JAL	CHECKED: JAL/BEC
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PROJECT TITLE:

WASHINGTON  
STREET  
SHERBORN  
HOMES

0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770

SHEET TITLE:

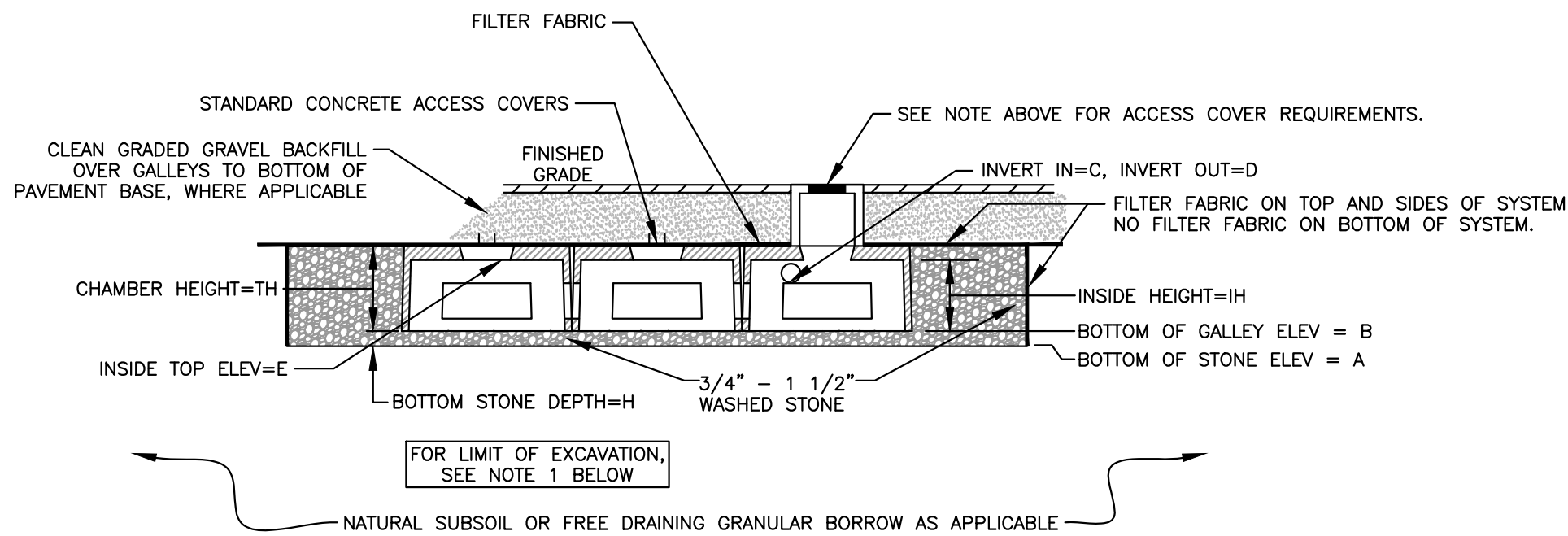
SITE DETAILS - 04

SHEET:  
11 OF 12

PROJECT NO.:  
F-25902

C-11

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SYSTEM	A	B	C	D	E	F	G	H	L	W	TH	IH	MODEL FOR SIZING
DETENTION #1	207.00	207.50	SEE PLAN	SEE PLAN	210.50	12"	12"	6"	72'	32'	3'-8"	3'-0"	RETAIN-IT 3' INSIDE HEIGHT CHAMBERS
DETENTION #2	205.00	205.50	SEE PLAN	SEE PLAN	210.00	12"	12"	6"	144'	16'	5'-2"	4'-6"	RETAIN-IT 4.5' INSIDE HEIGHT CHAMBERS

GENERAL NOTES:

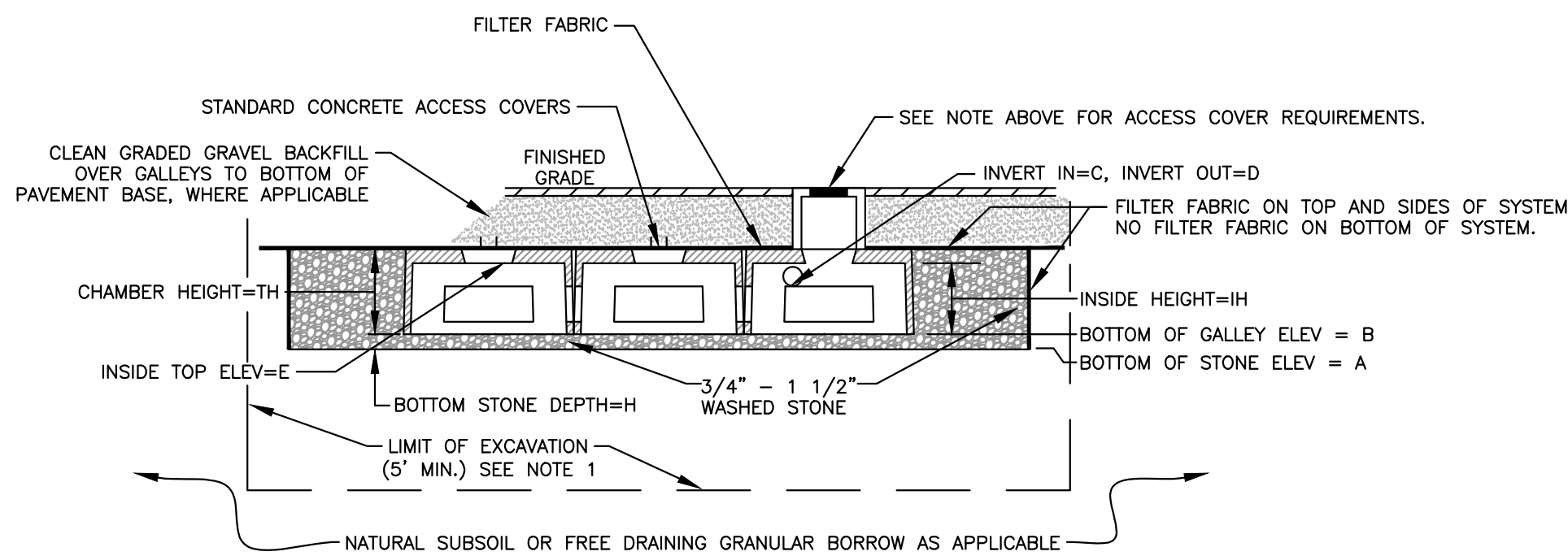
- ALL TOPSOIL, SUBSOIL AND DELETERIOUS MATERIAL, IF ANY, MUST BE EXCAVATED AND REMOVED TO THE HORIZONTAL AND VERTICAL (BOTTOM OF STONE ELEVATION) LIMITS OF THE DETENTION FACILITY.
- ALL WASHED STONE MUST HAVE LESS THAN 0.2 PERCENT MATERIAL FINER THAN A NUMBER 200 SIEVE AS DETERMINED BY THE A.A.S.H.T.O. TEST METHODS T-11 AND T-27 (LATEST EDITION).
- CONCRETE CHAMBERS SHALL BE DESIGNED FOR H-20 LOADING.

DETENTION #1 = 4 ROWS OF 9 CHAMBERS (36 TOTAL)  
SEE PLAN FOR LAYOUT

DETENTION #2 = 2 ROWS OF 18 CHAMBERS (36 TOTAL)  
SEE PLAN FOR LAYOUT

DETENTION SYSTEM #1 & #2 DETAIL

(NO SCALE)



SYSTEM	A	B	C	D	E	F	G	H	L	W	TH	IH	MODEL FOR SIZING
RECHARGER #1	207.10	207.60	SEE PLAN	SEE PLAN	212.60	24"	24"	6"	56'	32'	5'-8"	5'-0"	RETAIN-IT 5' INSIDE HEIGHT CHAMBERS
RECHARGER #2	207.00	207.50	SEE PLAN	SEE PLAN	211.50	24"	24"	6"	64'	32'	4'-8"	4'-0"	RETAIN-IT 4' INSIDE HEIGHT CHAMBERS

GENERAL NOTES:

- ALL TOPSOIL (A-HORIZON), SUBSOIL (B-HORIZON) AND DELETERIOUS MATERIAL, IF ANY, MUST BE EXCAVATED AND REMOVED BELOW THE BOTTOM ELEVATION OF THE LEACHING GALLEY, AND TO A DISTANCE OF 5 FEET FROM ALL SIDES OF THE LEACHING GALLEY. EXCAVATE DOWN TO 6 INCHES BELOW THE SURFACE OF THE NATURAL PERMEABLE SOIL (C HORIZON). BACKFILL AS REQUIRED WITH FREE DRAINING GRANULAR BORROW (AKA TITLE 5 SAND), FREE FROM FINES, CLAY, ORGANIC MATTER, AND LARGE BOULDERS.
- ALL WASHED STONE MUST HAVE LESS THAN 0.2 PERCENT MATERIAL FINER THAN A NUMBER 200 SIEVE AS DETERMINED BY THE A.A.S.H.T.O. TEST METHODS T-11 AND T-27 (LATEST EDITION).
- CONCRETE CHAMBERS SHALL BE DESIGNED FOR H-20 LOADING.

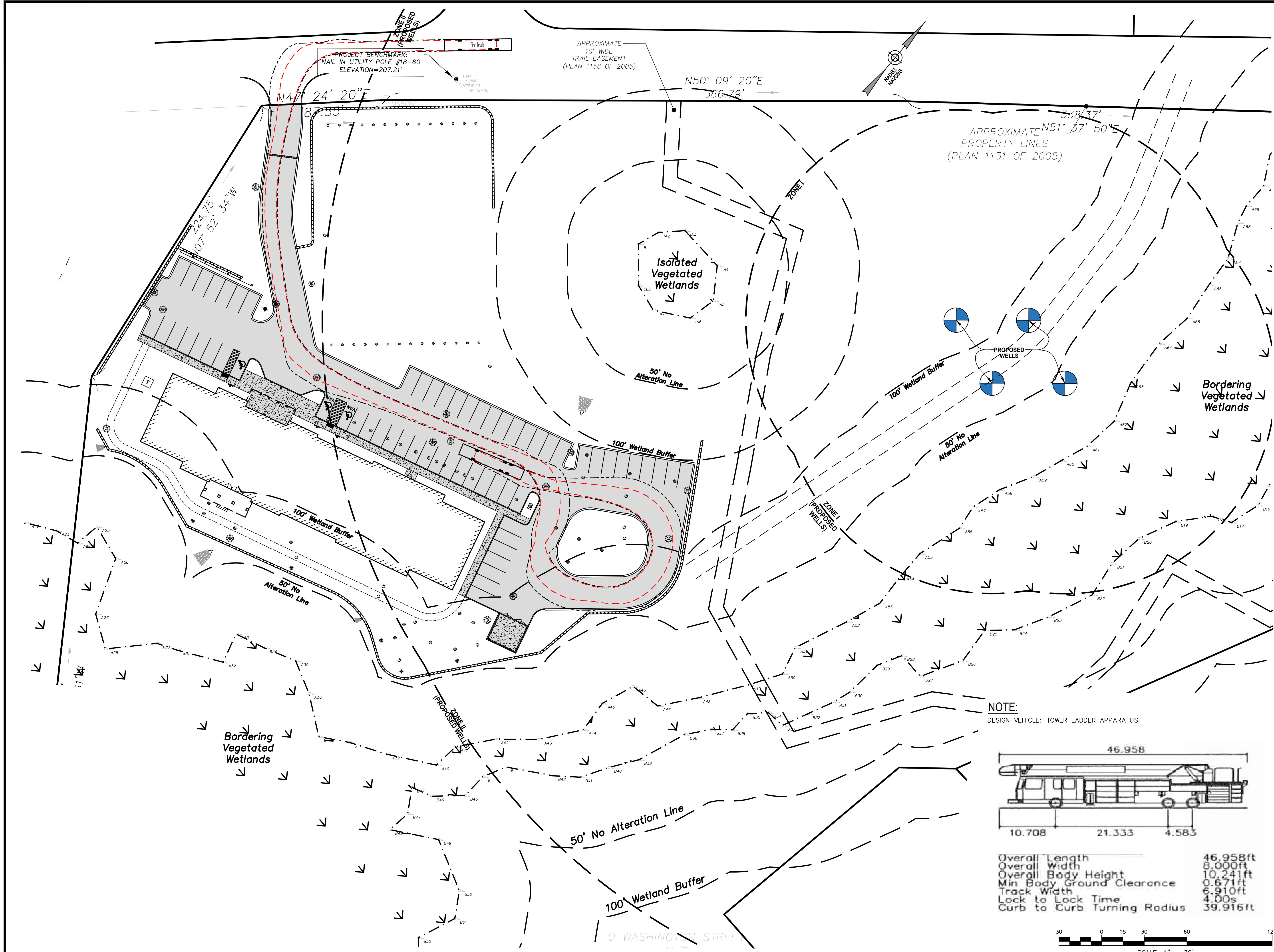
RECHARGER #1 = 4 ROWS OF 7 CHAMBERS (28 TOTAL)  
SEE PLAN FOR LAYOUT


RECHARGER #2 = 4 ROWS OF 8 CHAMBERS (32 TOTAL)  
SEE PLAN FOR LAYOUT

RECHARGER #1 AND #2 DETAIL

(NO SCALE)







**DGT Associates**  
Surveying & Engineering


**Framingham**  
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1071 Worcester Road  
Framingham, MA 01701  
508-879-0030

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PARCEL ID:  
**MAP 7, LOT 0, BLOCK 49**

ISSUED FOR:  
**COMPREHENSIVE PERMIT APPLICATION**



**BERT E. COREY**  
CIVIL  
No. 48423  
REGISTERED PROFESSIONAL ENGINEER

1	BEC	2/9/24	DESIGN DEVELOPMENT
NO.	APP	DATE	DESCRIPTION

DATE: **JANUARY 23, 2024**

SCALE: **1" = 30'**

DESIGN: <b>KMR</b>	DRAFTED: <b>KMR/JAL</b>	CHECKED: <b>JAL/BEC</b>
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PROJECT TITLE:  
**WASHINGTON STREET  
SHERBORN HOMES**

**0 WASHINGTON STREET  
SHERBORN, MASSACHUSETTS 01770**

SHEET TITLE:  
**SITE DETAILS - 05**

SHEET:  
**12 OF 12**

PROJECT NO.:  
**F-25902**

**C-12**

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