

PERMIT SITE PLAN
for
SHERBORN COTTAGE COURT
at
34 Brush Hill Road in Sherborn, MA

GENERAL NOTES

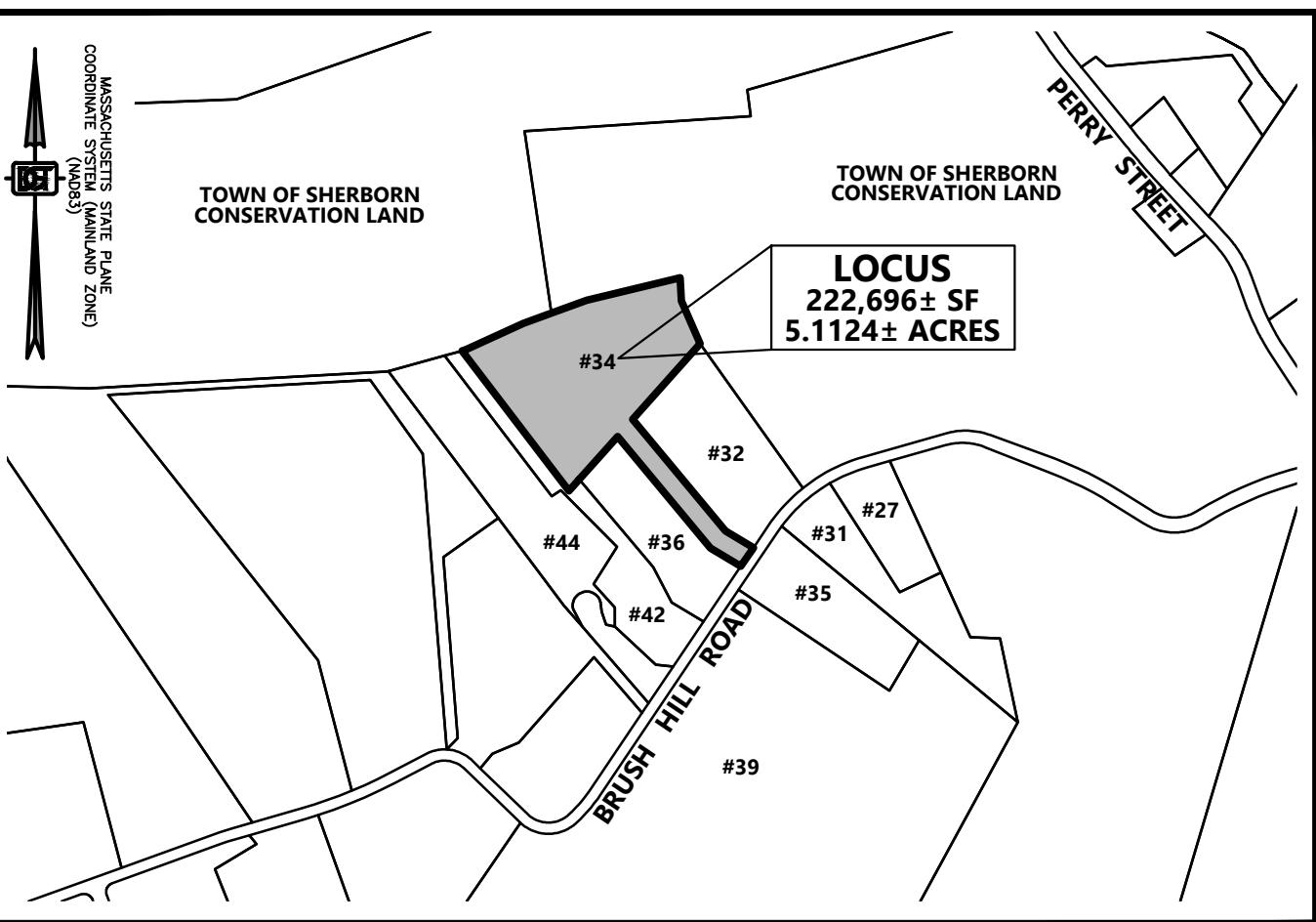
1. ELEVATIONS SHOWN HEREON REFER TO NAVD88.
2. TOPOGRAPHIC INFORMATION SHOWN HEREON WAS OBTAINED FROM SAMIOTES CONSULTANTS, INC.
3. THE CONTRACTOR SHALL VERIFY THE LOCATION AND RELATIVE ELEVATION OF BENCHMARKS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION. ANY DISCREPANCY SHALL BE REPORTED TO THE ENGINEER.
4. ALL SITE CONSTRUCTION SHALL COMPLY WITH THE TOWN OF SHERBORN DEPARTMENT OF PUBLIC WORKS.
5. IN CASES WHERE LEDGE, BURIED FOUNDATIONS OR BOULDERS ARE PRESENT, DGT ASSOCIATES SHALL NOT BE RESPONSIBLE FOR THE AMOUNT OF ROCK OR CONCRETE ENCOUNTERED.
6. DGT ASSOCIATES SHALL BE NOTIFIED OF ANY SIGNIFICANT DIFFERENCES IN THE EXISTING CONDITIONS OR UTILITIES THAT MAY AFFECT THE CONSTRUCTION SHOWN ON THIS PLAN FOR ANY NECESSARY PLAN REVISIONS.
7. THIS PLAN IS NOT INTENDED TO SHOW AN ENGINEERED BUILDING FOUNDATION DESIGN WHICH WOULD INCLUDE DETAILS AND ELEVATIONS FOR FOOTINGS, FOUNDATION WALL DESIGN AND ANY SUBSURFACE DRAINAGE TO PREVENT FLOODING. COORDINATE WITH THE ARCHITECTURAL AND STRUCTURAL PLANS.
8. THE PROPOSED BUILDING CONFIGURATION AS SHOWN HEREON SHALL BE CONSIDERED CONCEPTUAL AND SHALL BE VERIFIED WITH THE FINAL ARCHITECTURAL PLANS AND THE CURRENT ZONING ORDINANCES PRIOR TO CONSTRUCTION.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR AND/OR REPLACEMENT OF ANY EXISTING FEATURES DAMAGED DURING CONSTRUCTION THAT ARE NOT INTENDED FOR DEMOLITION AND/OR REMOVAL HEREON.
10. SAFETY MEASURES, CONSTRUCTION METHODS AND CONTROL OF WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

GRADING AND UTILITY PLAN NOTES

1. LOCATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN HEREON ARE APPROXIMATE ONLY. ALL UTILITIES, OBSTRUCTIONS AND/OR SYSTEMS MAY NOT BE SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR LOCATING AND PROTECTING ALL EXISTING UNDERGROUND UTILITIES AND/OR SYSTEMS WHETHER OR NOT SHOWN HEREON.
2. UNLESS OTHERWISE SHOWN HEREON, ALL NEW UTILITIES SHALL BE UNDERGROUND.
3. RIM ELEVATIONS SHOWN HEREON FOR NEW STRUCTURES ARE PROVIDED TO ASSIST THE CONTRACTOR WITH MATERIAL TAKEOFFS. FINAL RIM ELEVATIONS SHALL MATCH PAVEMENT, GRADING, LANDSCAPING, UNLESS SPECIFICALLY INDICATED OTHERWISE.
4. WHERE CONNECTIONS AND STRUCTURES ARE TO BE INSTALLED AT EXISTING UTILITIES, THE CONTRACTOR SHALL CONFIRM THE LOCATION AND ELEVATION PRIOR TO INSTALLATION AND SHALL REPORT ANY SIGNIFICANT DISCREPANCY FROM THE PLAN INFORMATION TO THE ENGINEER.
5. CURB RADII AND DIMENSIONS SHOWN HEREON ARE AT THE FACE OF CURB.
6. WHERE NEW PAVING MEETS EXISTING PAVING, MEET LINE AND GRADE OF EXISTING.
7. AT LOCATIONS WHERE EXISTING PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING PAVEMENT SHALL BE SAWCUT TO A CLEAN AND SMOOTH EDGE.
8. PERIMETER EROSION CONTROLS SHOWN HEREON SHALL BE INSTALLED PRIOR TO ANY EARTH DISTURBANCE AND SHALL SERVE AS A LIMIT OF WORK, UNLESS OTHERWISE SHOWN.
9. THE CONTRACTOR SHALL PROVIDE DUST CONTROL FOR CONSTRUCTION OPERATIONS AS APPROVED BY THE ENGINEER.
10. ALL POINTS OF CONSTRUCTION EGRESS OR INGRESS SHALL BE MAINTAINED TO PREVENT THE TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC ROADS.

REGULATORY NOTES

1. THE LOCATION OF ALL UNDERGROUND UTILITIES SHOWN HEREON ARE BASED ON FIELD LOCATION OF VISIBLE STRUCTURES AND COMPILED INFORMATION FROM PLANS PROVIDED BY UTILITY COMPANIES AND GOVERNMENT AGENCIES. THE LOCATION SHOWN HEREON SHALL BE CONSIDERED APPROXIMATE. BEFORE ANY CONSTRUCTION, DEMOLITION OR SITE WORK, THE LOCATION OF ALL UNDERGROUND UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR BY CONTACTING "DIG-SAFE" AT 811.
2. THE CONTRACTOR SHALL MAKE HIMSELF AWARE OF ALL CONSTRUCTION REQUIREMENTS, CONDITIONS AND LIMITATIONS IMPOSED BY PERMITS AND APPROVALS ISSUED BY REGULATORY AUTHORITIES PRIOR TO THE COMMENCEMENT OF ANY WORK. CONTRACTOR SHALL COORDINATE AND OBTAIN ALL CONSTRUCTION PERMITS REQUIRED BY REGULATORY AUTHORITIES.
3. ALL WORK OUTSIDE OF THE BUILDING THAT IS LESS THAN 10 FEET FROM THE INSIDE FACE OF THE BUILDING FOUNDATIONS SHALL CONFORM WITH THE UNIFORM STATE PLUMBING CODE OF MASSACHUSETTS, 248 CMR.
4. CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE RULES AND REGULATIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA).



SHEET INDEX

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Table 1. Summary of Project Site Conditions

General Site Conditions	Land Condition	Land Breakdown	Acres	Square Feet	Coverage (%)
		Total Area	5.1124	222,696	100.0%
		Unusable Land	Wetlands	0.0000	0.0%
	Usable Land	Upland	5.1124	222,696	100.0%
Existing Conditions	Disturbed	Total	0.0924	4,025	1.8%
	Impervious	Building	0.0000	0	0.0%
		Pavement	0.0000	0	0.0%
		Sidewalk	0.0000	0	0.0%
	Pervious	Lawn (usable open space)	0.0924	4,025	1.8%
	Undisturbed	Total	5.0200	218,671	98.2%
	Usable Open Space	Upland Woods	5.0200	218,671	98.2%
	Unusable Open Space	Wetlands	0.0000	0	0.0%
	Total Usable Open Space	Lawn / Woods	5.1124	222,696	100.0%
Proposed Conditions	Disturbed	Total	4.3658	190,176	85.4%
	Impervious	Building	0.3818	16,630	7.5%
		Pavement	0.6655	28,990	13.0%
		Sidewalk	0.0000	0	0.0%
	Pervious	Lawn (usable open space)	3.3185	144,556	64.9%
	Undisturbed	Total	0.7466	32,520	14.6%
	Usable Open Space	Upland Woods	0.7466	32,520	14.6%
	Unusable Open Space	Wetlands	0.0000	0	0.0%
	Total Usable Open Space	Lawn / Woods	4.0651	177,076	79.5%

Table 2. Summary of Proposed Buildings

Item	Total	2 Bedroom	3 Bedroom
Units	8	6	2
Bedrooms	18	12	6

Table 3. Zoning Summary Table (Sherborn Residential B District)

Item	Required		Proposed Conditions		Waiver Required
Minimum Lot Area	2	acres	5.1124	acres	No
Minimum Frontage	200	feet	50.02	feet	Yes
Minimum Lot Width	200	feet	50.02	feet	Yes
Minimum Lot Depth	N/A		N/A		N/A
Minimum Front Setback	60	feet	513.5	feet	No
Minimum Side Setback	40	feet	50.8	feet	No
Minimum Rear Setback	30	feet	51.5	feet	No
Maximum Height (stories)	2.5	stories	2	stories	No
Maximum Height (feet)	35	feet	> 35	feet	No
Maximum Lot Coverage	N/A		N/A		N/A
Lot Coverage					
Building	7.5%				
Parking / Paved Areas	13.0%				
Usable Open Space	79.5%				
Unusable Open Space (wetlands)	0.0%				
Lot Coverage	20.5%				

DGT Associates
Surveying &
Engineering

Framingham
Boston • Worcester • Preston, CT

1071 Worcester Road
Framingham, MA 01701
508-879-0030

www.DGTassociates.com

PARCEL ID:

NO.	APP	DATE	DESCRIPTION
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DATE: JUNE 4, 2024	SCALE: AS NOTED	DESIGN: KMR	DRAFTED: KMR	CHECKED: BEC
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SHERBORN COTTAGE COURT

**34 BRUSH HILL ROAD
SHERBORN MA 01770**

SHEET TITLE:

TITLE SHEET

SHEET:
1 OF 12

PROJECT NO.:
F-25889

C-1

EROSION AND SEDIMENT CONTROL NOTES

1. CONSTRUCTION PERIOD EROSION AND SEDIMENT CONTROL

1.1 THE PURPOSE OF THE CONSTRUCTION PERIOD EROSION AND SEDIMENT CONTROL PLAN IS TO MINIMIZE THE INTRODUCTION OF SEDIMENTS INTO PUBLIC RIGHT OF WAYS, ABUTTING PROPERTIES, AND TO POST-DEVELOPMENT STORMWATER BMP'S RESULTING FROM THE LAND DISTURBANCE ACTIVITIES DURING CONSTRUCTION.

1.2 INSPECTIONS SHALL BE CONDUCTED BY THE GENERAL CONTRACTOR ON A BI-WEEKLY BASIS (EVERY TWO WEEKS), OR FOLLOWING SIGNIFICANT STORM EVENTS (RAINFALL OF 0.5" OR MORE) THAT CAN AFFECT THE SEDIMENT AND EROSION CONTROL PRACTICES IMPLEMENTED AT THE SITE. THE PURPOSE OF THE INSPECTIONS IS TO EVALUATE THE EFFECTIVENESS OF THE CONTROLS AND ANY REQUIRED MAINTENANCE ACTIVITIES. IF AN EROSION/SEDIMENTATION CONTROL MEASURE IS FOUND TO BE INADEQUATE FOR PROPERLY CONTROLLING SEDIMENT, AN ADEQUATE MEASURE SHALL BE DESIGNED AND IMPLEMENTED. A COPY OF THE WRITTEN INSPECTION SHALL BE KEPT ON FILE AT THE CONSTRUCTION SITE.

1.3 DURING CONSTRUCTION, PROPOSED STORMWATER MANAGEMENT STRUCTURES THAT INFILTRATE RUNOFF ARE PARTICULARLY SENSITIVE TO DAMAGE BY SEDIMENT. INFILTRATION TECHNOLOGIES ARE NOT DESIGNED TO HANDLE THE HIGH CONCENTRATIONS OF SEDIMENTS TYPICALLY FOUND IN CONSTRUCTION RUNOFF, AND MUST BE PROTECTED FROM CONSTRUCTION RELATED SEDIMENT. THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURES TO PROTECT THESE SYSTEMS: PROTECT INFILTRATION SYSTEMS, TRENCH DRAIN, AND CATCH BASINS UNTIL THE TRIBUTARY DRAINAGE AREA IS STABLE OR THE RUNOFF IS TREATED TO BE ESSENTIALLY FREE FROM SEDIMENT TO THE SATISFACTION OF THE ENGINEER. THE CONTRACTOR SHALL PROVIDE TEMPORARY BY-PASS SYSTEMS AS NECESSARY TO PREVENT CONSTRUCTION SITE RUNOFF FROM ENTERING THE INFILTRATION SYSTEMS. THE INFILTRATION SYSTEMS SHALL REMAIN OFF-LINE AND PROTECTED. CLEAN ROOF RUNOFF MAY DISCHARGE INTO THE INFILTRATION SYSTEMS IF IT IS PIPED DIRECTLY TO THE SYSTEMS AND NOT DIRECTED OVER DISTURBED AREAS.

1.4 NO STOCKPILING IS ALLOWED WITHIN THE FOOTPRINT OF THE PROPOSED INFILTRATION SYSTEMS OR THE FOOTPRINT OF THE PROPOSED SOIL ABSORPTION FIELD. CONTRACTOR IS TO LOCATE THE PROPOSED AREAS FOR THESE SYSTEMS PRIOR TO THE START OF CONSTRUCTION.

1.5 DURING THE INSTALLATION OF THE INFILTRATION SYSTEMS, NO PARKING WILL BE PERMITTED OVER THE SYSTEM UNTIL IT HAS BEEN PROPERLY BACKFILLED AND PROTECTED FROM SEDIMENT AND VEHICULAR LOADS.

1.6 NO PARKING IS ALLOWED OVER THE FOOTPRINT OF THE PROPOSED SOIL ABSORPTION SYSTEM AT ANYTIME DURING THE CONSTRUCTION PROCESS.

2. GENERAL PERFORMANCE STANDARDS:

2.1 THE CONTRACTOR SHALL INSTALL, ROUTINELY INSPECT AND MAINTAIN ALL SEDIMENT AND EROSION CONTROLS SUCH THAT THEY ARE IN PROPER WORKING ORDER AT ALL TIMES DURING THE CONSTRUCTION PROJECT UNTIL SUCH TIME AS ALL AREAS OF THE SITE TRIBUTARY TO THOSE EROSION CONTROLS ARE IN A PERMANENTLY STABILIZED CONDITION.

2.2 THE CONTRACTOR SHALL MANAGE THE SITE SUCH THAT EROSION AND SEDIMENT FROM RUNOFF AND WIND BLOWN DUST ARE CONTROLLED AND MINIMIZED AT ALL TIMES. THE EROSION CONTROLS SHOWN ON THIS PLAN INCLUDE THE INITIAL SET OF EROSION CONTROLS AND BASIC FORMATION OF THE SITE TO MEET THE REQUIREMENTS OF BEST MANAGEMENT PRACTICES. THE CONTRACTOR SHALL MANAGE THE SITE PERIODICALLY WHICH MAY NOT BE STABILIZED AREAS OF EXPOSED SOILS; INSTALL TEMPORARY COVER; MAKE NECESSARY ADJUSTMENTS TO THE EROSION CONTROL INSTALLATIONS TO IMPROVE FUNCTION; PROVIDE TEMPORARY SEDIMENT BASINS; INSTALL ADDITIONAL EROSION CONTROL WHERE NECESSARY.

2.3 THE EROSION CONTROL WORK SHOWN ON THIS PLAN ARE ALSO SUBJECT TO OTHER STATE AND LOCAL APPROVALS. THE CONTRACTOR SHALL OBTAIN AND BE RESPONSIBLE FOR COMPLIANCE WITH THE CONDITIONS AND REQUIREMENTS OF THOSE PERMITS.

2.4 DESIGN, INSTALLATION AND MAINTENANCE OF SEDIMENT AND EROSION CONTROLS SHALL BE IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES AS FLOWING: -"CONSTRUCTION EROSION AND SEDIMENT CONTROL PRACTICES", U.S. ENVIRONMENTAL PROTECTION AGENCY, OCTOBER 1992; -"MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES FOR URBAN AND SUBURBAN AREAS, A GUIDE FOR PLANNERS, DESIGNERS AND MUNICIPAL OFFICIALS", MASS. EXECUTIVE OFFICE OF ENVIRONMENTAL AFFAIRS, MAY 2003. - U.S.D.A. NATURAL RESOURCES AND CONSERVATION SERVICES (NRCS) GUIDELINES.

3. FEDERAL NPDES PHASE II COMPLIANCE:

3.1 THIS PROJECT IS SUBJECT TO THE FEDERAL CLEAN WATER ACT REQUIREMENTS FOR CONSTRUCTION SITES ADMINISTERED BY THE U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA). THIS PROGRAM IS THE "NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) - PHASE II FOR CONSTRUCTION SITES FOR COMPLIANCE WITH THIS PROGRAM, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A COMPLETE "STORMWATER POLLUTION PREVENTION PLAN" (SWPPP) IN CONFORMANCE WITH THE CURRENT CONSTRUCTION GENERAL PERMIT AND FILING A NOTICE OF INTENT WITH THE EPA UNDER THE NPDES PROGRAM. THE CONTRACTOR SHALL THEN MANAGE THE SITE IN COMPLIANCE WITH THE SWPPP AND IN COMPLIANCE WITH THE CONSTRUCTION GENERAL PERMIT. NOTE THAT THE SWPPP IS TO BE CONSISTENT WITH THIS EROSION AND SEDIMENT CONTROL PLAN. THE CONTRACTOR SHALL PREPARE A CONSTRUCTION GENERAL PERMIT. THE CONSTRUCTION GENERAL PERMIT SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE SWPPP. THIS EROSION AND SEDIMENT CONTROL PLAN INCLUDED IN THIS PLAN SET MAY BE USED AS PART OF THE DOCUMENTATION REQUIRED FOR THE PREPARATION OF A SWPPP, BUT IS NOT TO BE CONSIDERED AS MEETING THE FULL REQUIREMENTS OF A SWPPP PREPARED FOR COMPLIANCE WITH THE NPDES PROGRAM.

4. PERIMETER FIBER LOG / SILT FENCING AND LIMIT OF WORK:

4.1 PRIOR TO ANY DISTURBANCE OR ALTERATIONS OF ANY AREA ON THE SITE, AN EROSION CONTROL BARRIER AND CONSTRUCTION FENCE SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PLAN.

4.2 INSTALL THE FIBER LOGS ON THE UPHILL SIDE OF THE SILT FENCE AS SHOWN. IN THOSE AREAS WHERE THE TOPOGRAPHY INDICATES THAT STORMWATER RUNOFF WILL BE CONCENTRATED (AT LOW POINTS), ADDITIONAL FIBER LOGS, AS NECESSARY, SHALL BE STAKED ON THE UPGRADE SIDE OF THE SEDIMENT BARRIER FOR ADDED FILTRATION AND PROTECTION. THE REQUIRED LOCATIONS FOR THE ADDITIONAL FIBER LOG INSTALLATION WILL BE SELECTED BY THE ENGINEER AND / OR THE AUTHORIZED INSPECTOR / SWPPP COORDINATOR UPON COMPLETION OF THE SEDIMENT BARRIER INSTALLATION. (SEE DETAIL).

4.3 ONCE INSTALLED, THE STAKED FIBER LOGS AND CONSTRUCTION FENCE SHALL BE MAINTAINED IN PLACE UNTIL ALL AREAS UPGRADED FROM THE BARRIERS HAVE BEEN PERMANENTLY STABILIZED.

4.4 ALL DISTURBED AREAS NOT OTHERWISE DEVELOPED OR WHERE SPECIAL STABILIZATION MEASURES OR LANDSCAPE PLANTINGS ARE PROPOSED SHALL BE LOAMED AND SEDED. SIX INCHES OF LOAM TOPSOIL (MIN. COMPACTED DEPTH) SHALL BE APPLIED UNLESS, OTHERWISE SPECIFIED. (SEE LANDSCAPE PLAN AND OTHER PLANS AS APPLICABLE).

4.5 THE PERIMETER FIBER LOGS AND CONSTRUCTION FENCE ARE ALSO LIMITS OF WORK. ALL AREAS OUTSIDE THE LIMIT ARE TO BE LEFT UNDISTURBED. DURING THE SITE WORK, ALL PERSONS AND EQUIPMENT SHALL STAY OUT OF THESE AREAS TO PRESERVE THE EXISTING VEGETATION AND SOIL COVER.

5. CONSTRUCTION ENTRANCE:

5.1 AT THE START OF SITE WORK, A STONE CONSTRUCTION ENTRANCE SHALL BE INSTALLED AT THE ACCESS TO THE SITE FROM THE ACCESS EASEMENT DRIVEWAY TO CONTROL THE TRACKING OF MUD OFF THE SITE. THE ENTRANCE SHALL BE MAINTAINED UNTIL THE SITE IS IN A STABILIZED CONDITION WHEN THE POSSIBILITY OF VEHICLES TRACKING MUD OFF SITE HAS BEEN ELIMINATED.

5.2 THE CONTRACTOR SHALL SWEEP THE ADJACENT ROADWAYS WHEN MUD, DUST, DIRT, DEBRIS, ETC. HAS SHOWN SIGNS OF BUILDUP ON THE ROADWAYS EXITING THE SITE. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO THIS MATTER AND IMMEDIATE ATTENTION IS ALWAYS REQUIRED.

6. DEWATERING OF EXCAVATIONS:

6.1 DISCHARGE FROM DEWATERING PUMPS OR TEMPORARY TRENCH OR EXCAVATION DRAINS SHALL NOT BE DISCHARGED DIRECTLY TO THE ON-SITE DRAINAGE SYSTEM, WETLANDS, OR SEWAGE DISPOSAL SYSTEM. DISCHARGES SHALL BE DIRECTED TO A TREATMENT SYSTEM CONSISTING OF A SEDIMENT BASIN, STRAW BALE SEDIMENT BASIN, FILTER BAG SYSTEM OR OTHER APPROVED METHOD TO FILTER THE DISCHARGE WATER AND PREVENT EROSION.

6.2 ALL DEWATERING DRAINS ASSOCIATED WITH CONSTRUCTION ACTIVITIES SHALL COMPLY WITH THE TOWN OF SHERBORN REQUIREMENTS, UNITED STATES ENVIRONMENTAL PROTECTION AGENCY, MASSACHUSETTS DEP AND OTHER APPROPRIATE AGENCIES. UNDER NO CIRCUMSTANCE SHALL DEWATERING DRAINAGE BE DISCHARGED INTO SANITARY SEWER.

7. SOIL STOCKPILES:

7.1 STOCKPILES OF SOIL MATERIALS SHALL BE PLACED WITHIN AREAS THAT ARE PROTECTED BY PERIMETER EROSION CONTROLS, OR SHALL BE SURROUNDED BY PROPER SILT FENCE OR FIBER LOGS.

7.2 STOCKPILES THAT ARE TO BE IN PLACE FOR EXTENDED PERIODS OF TIME (MORE THAN 30 DAYS) SHALL BE COVERED OR OTHERWISE TEMPORARILY STABILIZED IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES.

8. CATCH BASIN INLET PROTECTION

8.1 CATCH BASINS AND TRENCH DRAINS WITHIN THE WORK AREA OR THAT WILL RECEIVE RUNOFF FROM THE WORK AREA SHALL BE PROTECTED WITH A SILT SACK AND OR OTHER APPROVED INSTALLATION TO MINIMIZE THE SEDIMENT LOAD TO THE CATCH BASIN.

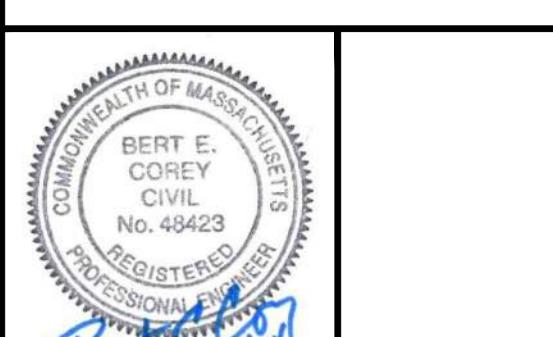
9. DUST CONTROL:

9.1 THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES DURING SITE WORK TO MINIMIZE WIND BLOWN DUST FROM EXPOSED SOIL SURFACES. MEASURES INCLUDE BUT ARE NOT LIMITED TO:

- SPRINKLING WATER ON EXPOSED SURFACES
- APPLICATION OF TEMPORARY COVER SUCH AS HYDRO MULCH AND TACKIFIER, STRAW MATTING, JUTE NETTING ETC.

PARCEL ID:
MAP 1, LOT 0, BLOCK 18

ISSUED FOR:
COMPREHENSIVE PERMIT APPLICATION



NO. APP. DATE DESCRIPTION
JUNE 4, 2024

SCALE: 1" = 30'

DESIGN: KMR DRAFTED: KMR CHECKED: BEC

PROJECT TITLE:

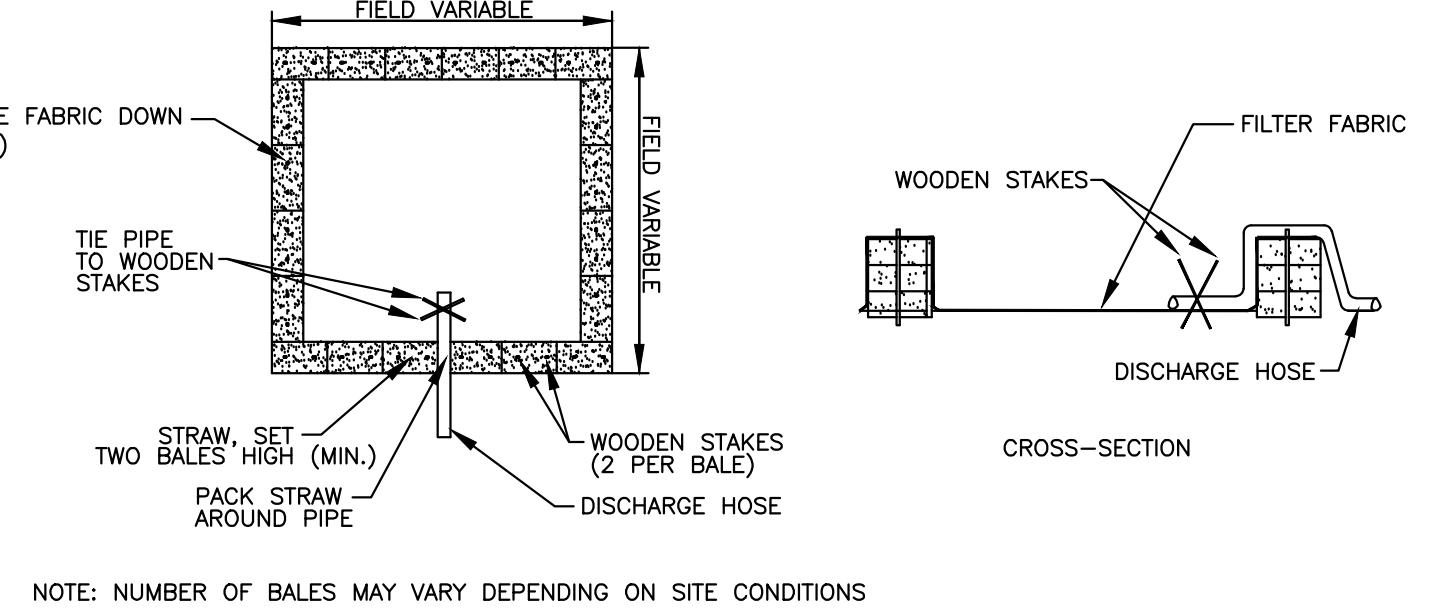
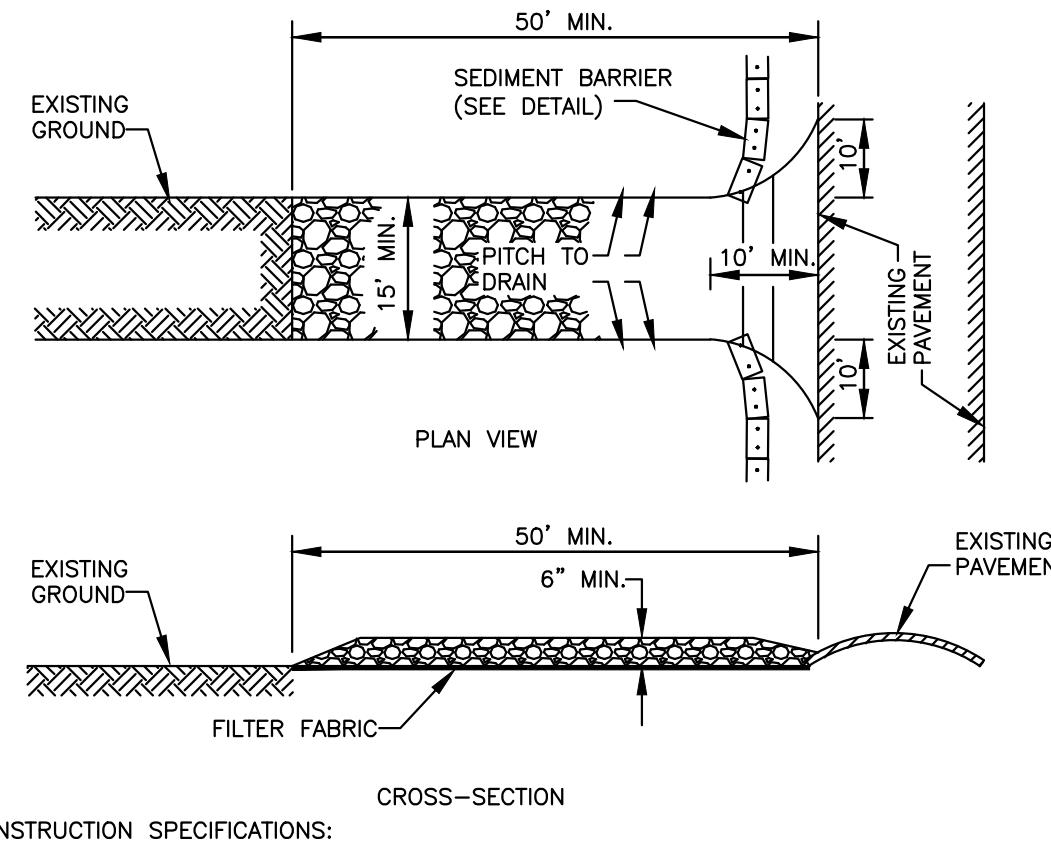
SHERBORN COTTAGE COURT

34 BRUSH HILL ROAD
SHERBORN, MA 01770

SHEET TITLE:

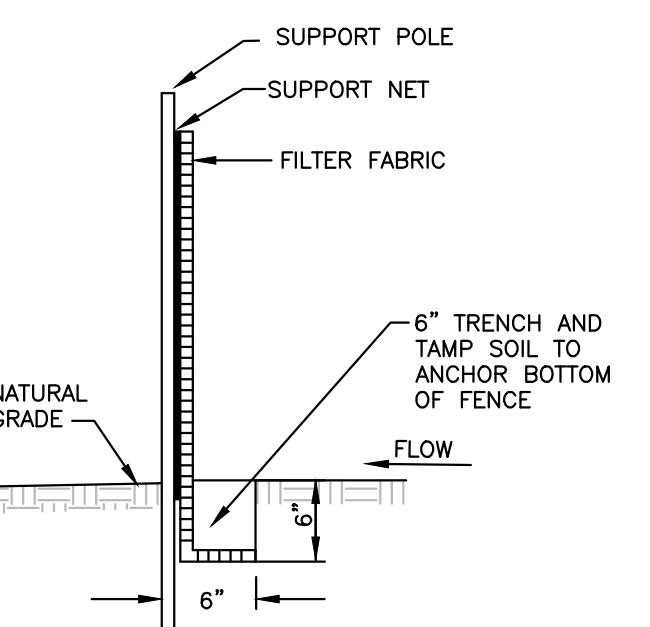
EROSION & SEDIMENT CONTROL PLAN & DETAILS - 02

SHEET:
4 OF 12
 PROJECT NO.:
F-25889
 C-4



STRAW BALE SETTLING BASIN

(NO SCALE)

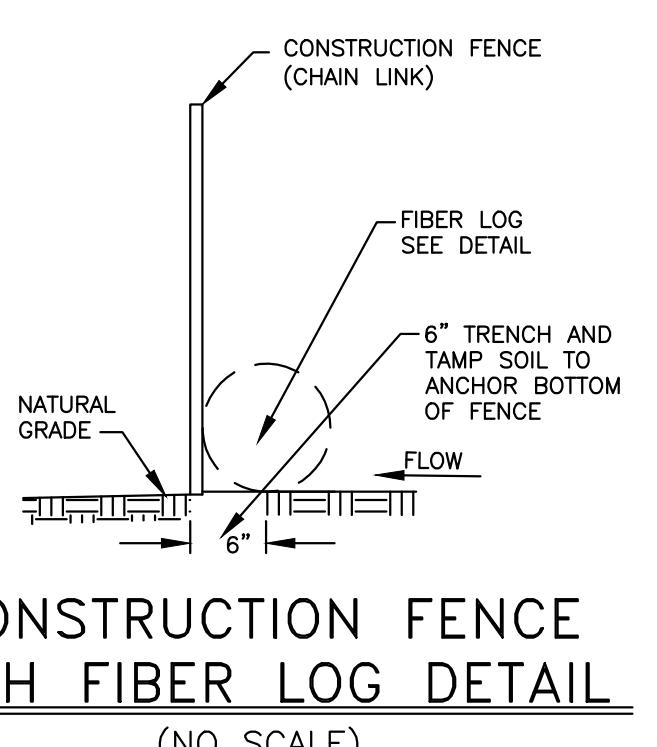


SILT FENCE INSTALLATION

(NO SCALE)

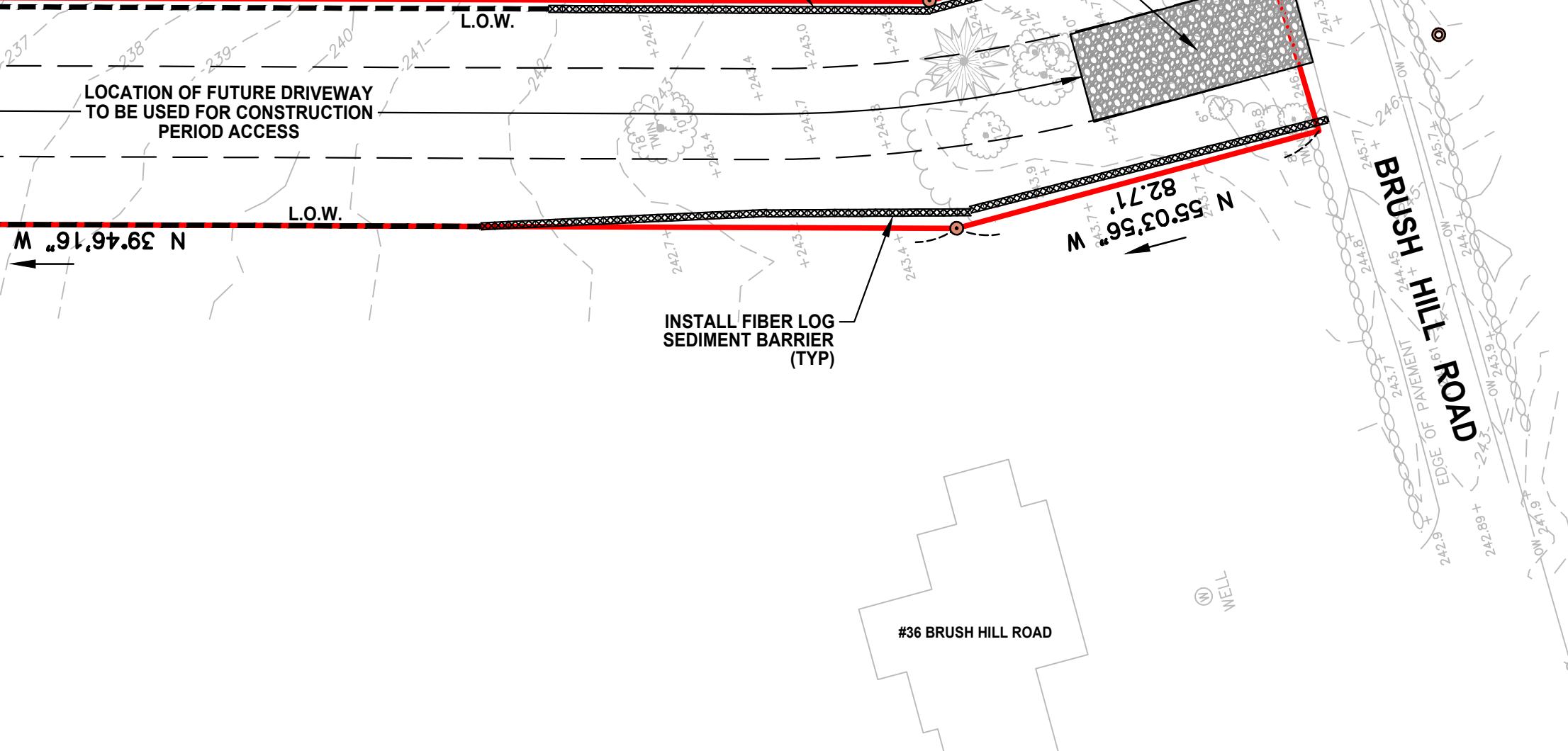
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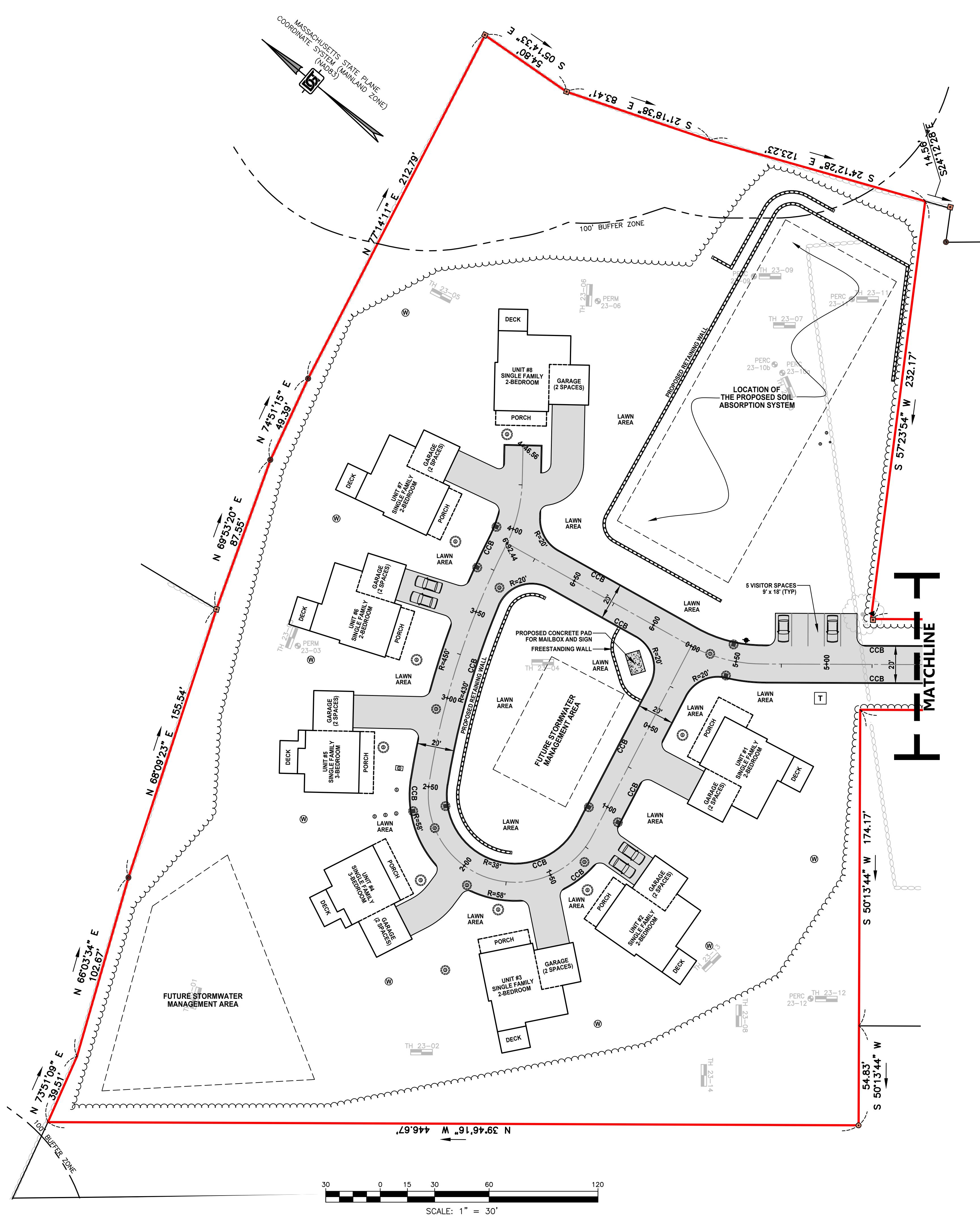
(NO SCALE)



MATCHLINE

SCALE: 1" = 30'
 30 0 15 30 60 120





LEGEND

- PROPOSED DRAINAGE STRUCTURE
- PROPOSED DEEP SUMP CATCH BASIN
- PROPOSED AREA DRAIN
- PROPOSED SEWER MANHOLE
- RADIUS = 20 FEET
- PROPOSED CAPE COD BERM
- PROPOSED VERTICAL GRANITE CURB
- HDPE FLARED END SECTION

BITUMINOUS CONCRETE PAVEMENT

CONCRETE PAVEMENT



BERT E.
COREY
CIVIL
NO. 48423

PARCEL ID:
MAP 1, LOT 0, BLOCK 18

ISSUED FOR:
**COMPREHENSIVE
PERMIT APPLICATION**

NO. APP	DATE	DESCRIPTION
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DATE: **JUNE 4, 2024**

SCALE: **1" = 30'**

DESIGN: **KMR** DRAFTED: **KMR** CHECKED: **BEC**

PROJECT TITLE:
**SHERBORN
COTTAGE
COURT**

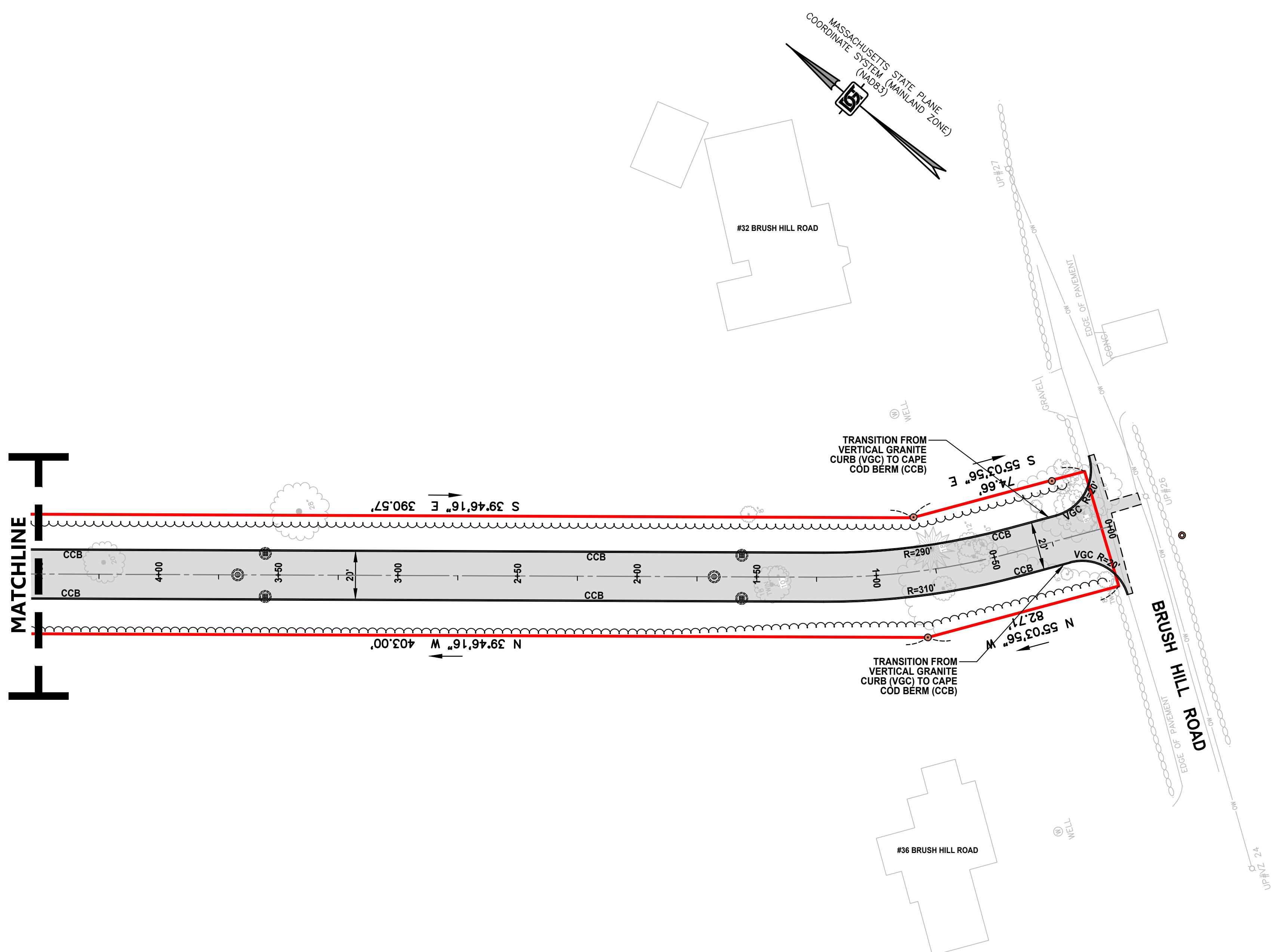
34 BRUSH HILL ROAD
SHERBORN, MA 01770

SHEET TITLE:

**AYOUT AND
MATERIALS PLAN - 01**

SHEET: **5 OF 12**
PROJECT NO.: **F-25889**

C-5



SHERBORN COTTAGE COURT

**34 BRUSH HILL ROAD
SHERBORN, MA 01770**

LAYOUT AND MATERIALS PLAN - 02

EET:
OF 12
CT NO.:
5889

C-6

ARCEL ID:

ISSUED FOR:

A circular seal for a civil engineer. The outer ring contains the text "CIVIL", "No. 48423", "REGISTERED", "PROFESSIONAL ENGINEER", and "S". The inner circle contains the signature "Bert E. Johnson" and the date "6/4/2011".

O.	APP	DATE	DESCRIPTION
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DATE: **JUNE 4, 2024**

CALE: **1" = 30'**

DESIGN: KMR	DRAFTED: KMR	CHECKED: BEC
PROJECT TITLE		

SHERBORN COTTAGE COURT

**34 BRUSH HILL ROAD
SHERBORN, MA 01770**

LEGEND

PROPOSED CONTOUR ELEVATION
PROPOSED DRAIN LINE
PROPOSED ROOF DRAIN
PROPOSED DRAINAGE STRUCTURE
PROPOSED DEEP SUMP CATCH BASIN
PROPOSED AREA DRAIN
PROPOSED SEWER MANHOLE
PROPOSED SPOT ELEVATION
PROPOSED TOP OF CURB ELEVATION
PROPOSED BOTTOM OF CURB ELEVATION
PROPOSED TOP OF WALL ELEVATION
PROPOSED BOTTOM OF WALL ELEVATION

HEET TITLE:

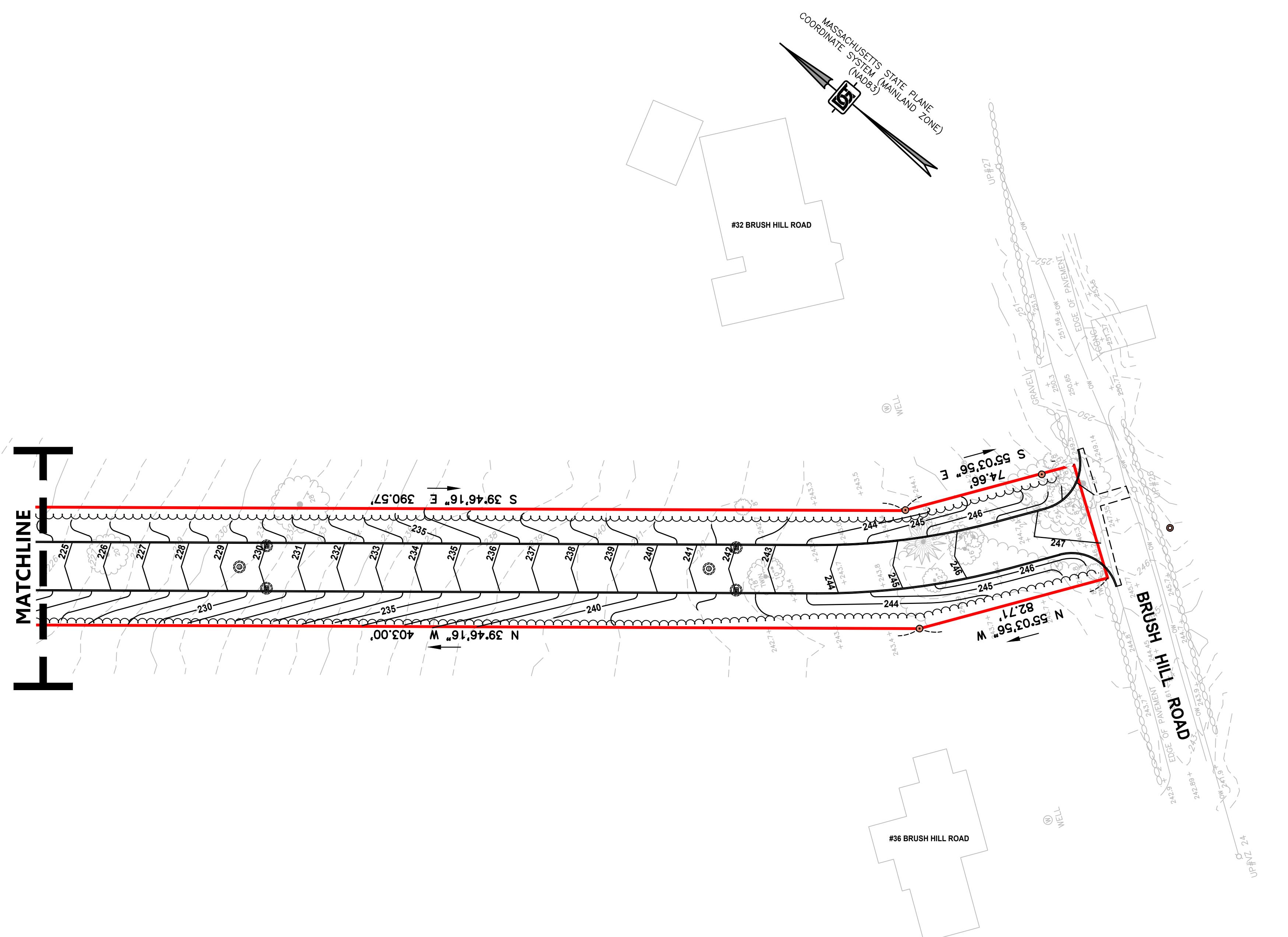
SITE GRADING PLAN - 01

SHEET:
7 OF 12

PROJECT NO.:
F-25889

C-7

<img alt="A detailed site plan for a residential development, likely a subdivision. The plan shows various lots, roads, and building footprints. A red line, labeled 'MATCHLINE', runs diagonally across the site. The plan includes contour lines, property boundaries, and specific lot numbers. Key features include a '100' BUFFER ZONE, several single-family homes (e.g., UNIT #5, #6, #8, #9, #10, #11, #12, #13, #14, #15, #16, #17, #18, #19, #20, #21, #22, #23, #24, #25, #26, #27, #28, #29, #30, #31, #32, #33, #34, #35, #36, #37, #38, #39, #40, #41, #42, #43, #44, #45, #46, #47, #48, #49, #50, #51, #52, #53, #54, #55, #56, #57, #58, #59, #60, #61, #62, #63, #64, #65, #66, #67, #68, #69, #70, #71, #72, #73, #74, #75, #76, #77, #78, #79, #80, #81, #82, #83, #84, #85, #86, #87, #88, #89, #90, #91, #92, #93, #94, #95, #96, #97, #98, #99, #100, #101, #102, #103, #104, #105, #106, #107, #108, #109, #110, #111, #112, #113, #114, #115, #116, #117, #118, #119, #120, #121, #122, #123, #124, #125, #126, #127, #128, #129, #130, #131, #132, #133, #134, #135, #136, #137, #138, #139, #140, #141, #142, #143, #144, #145, #146, #147, #148, #149, #150, #151, #152, #153, #154, #155, #156, #157, #158, #159, #160, #161, #162, #163, #164, #165, #166, #167, #168, #169, #170, #171, #172, #173, #174, 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LEGEND

—123—	PROPOSED CONTOUR ELEVATION
—D—	PROPOSED DRAIN LINE
—RD—	PROPOSED ROOF DRAIN
(D)	PROPOSED DRAINAGE STRUCTURE
(■)	PROPOSED DEEP SUMP CATCH BASIN
(●)	PROPOSED AREA DRAIN
(S)	PROPOSED SEWER MANHOLE
+123.4	PROPOSED SPOT ELEVATION
T.C.=123.4	PROPOSED TOP OF CURB ELEVATION
B.C.=123.4	PROPOSED BOTTOM OF CURB ELEVATION
T.W.=123.4	PROPOSED TOP OF WALL ELEVATION
B.W.=123.4	PROPOSED BOTTOM OF WALL ELEVATION

+123.4	PROPOSED SPOT ELEVATION
T.C.=123.4	PROPOSED TOP OF CURB ELEVATION
B.C.=123.4	PROPOSED BOTTOM OF CURB ELEVATION
T.W.=123.4	PROPOSED TOP OF WALL ELEVATION
B.W.=123.4	PROPOSED BOTTOM OF WALL ELEVATION

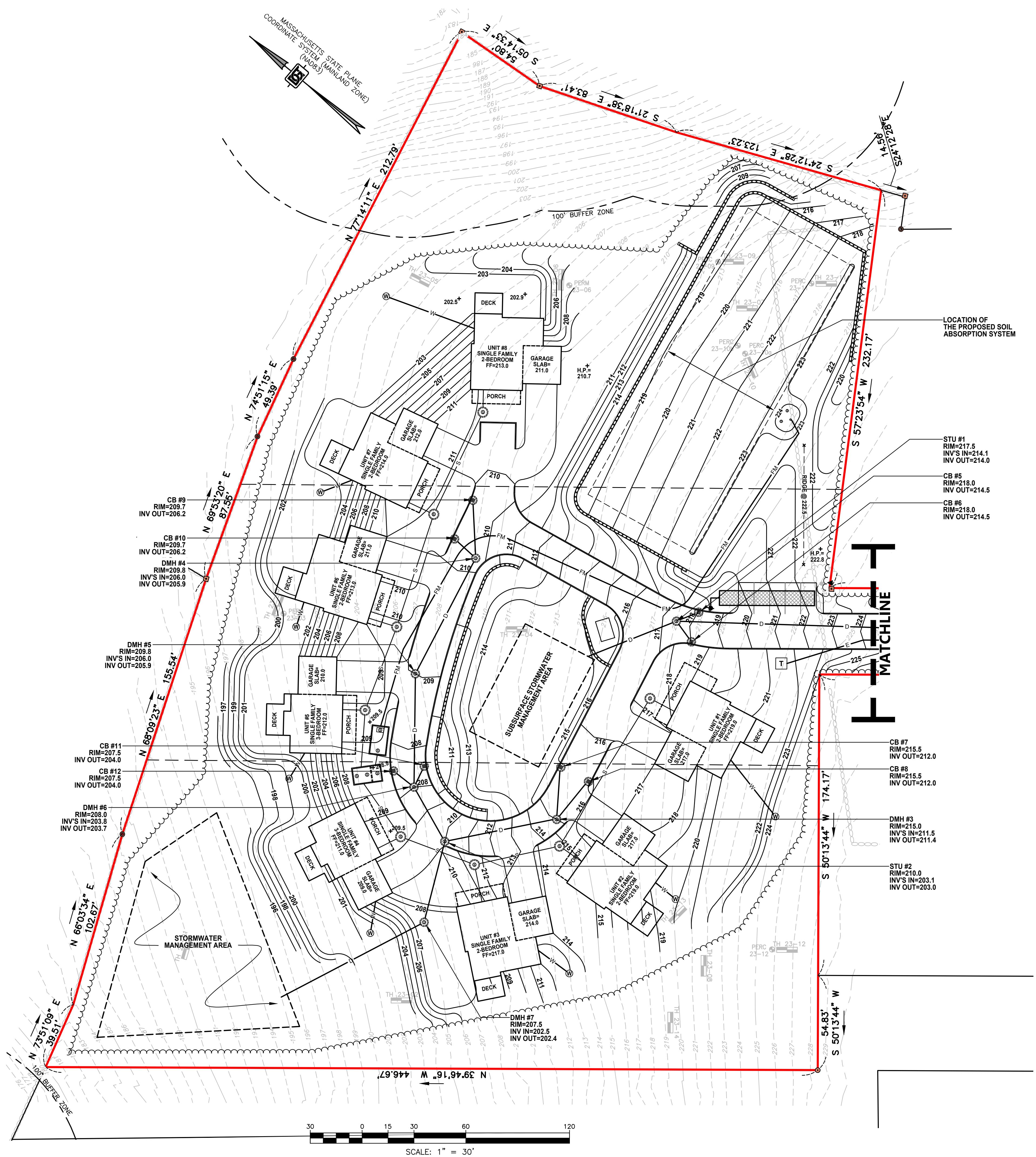
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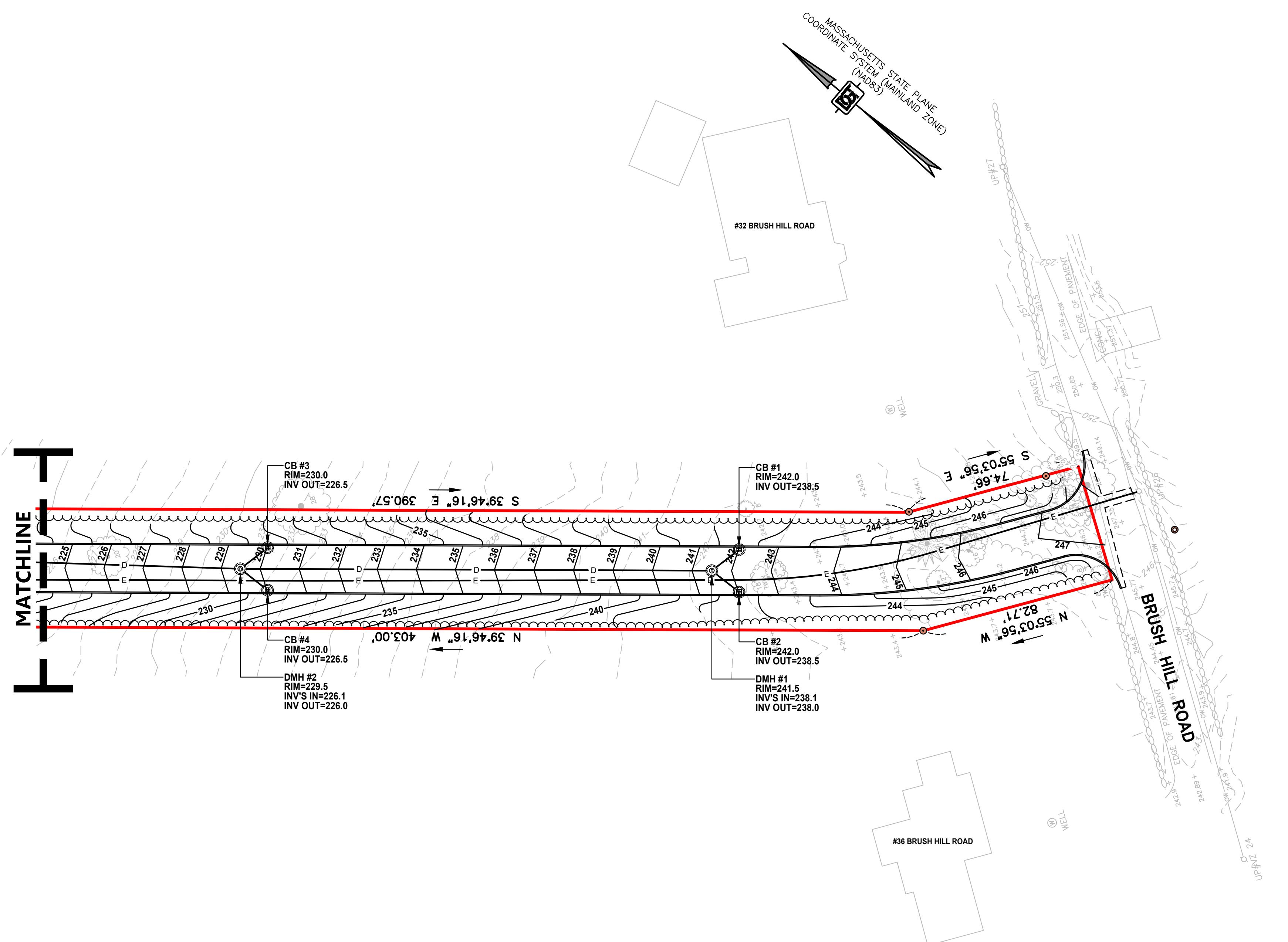
SITE GRADING PLAN - 02

T:
12
NO.:

SCALE: 1" = 30'

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LEGEND

123	PROPOSED CONTOUR ELEVATION
D	PROPOSED DRAIN LINE
RD	PROPOSED ROOF DRAIN
DMH (D)	PROPOSED DRAIN MANHOLE
STU (D)	PROPOSED STORMWATER TREATMENT UNIT
CB (S)	PROPOSED DEEP SUMP CATCH BASIN
AD (S)	PROPOSED AREA DRAIN
OCS (D)	PROPOSED OUTLET CONTROL STRUCTURE
SMH (S)	PROPOSED SEWER MANHOLE
HDPE	HIGH DENSITY POLYETHYLENE
+123.4	PROPOSED SPOT ELEVATION
T.C.=123.4	PROPOSED TOP OF CURB ELEVATION
B.C.=123.4	PROPOSED BOTTOM OF CURB ELEVATION
T.W.=123.4	PROPOSED TOP OF WALL ELEVATION
B.W.=123.4	PROPOSED BOTTOM OF WALL ELEVATION

LEGEND
PROPOSED CONTOUR ELEVATION

34 BRUSH HILL ROAD
SHERBORN, MA 01770

Sherborn

SET TITLE:

DRAINAGE PLAN - 02

SHEET:
10 OF 12

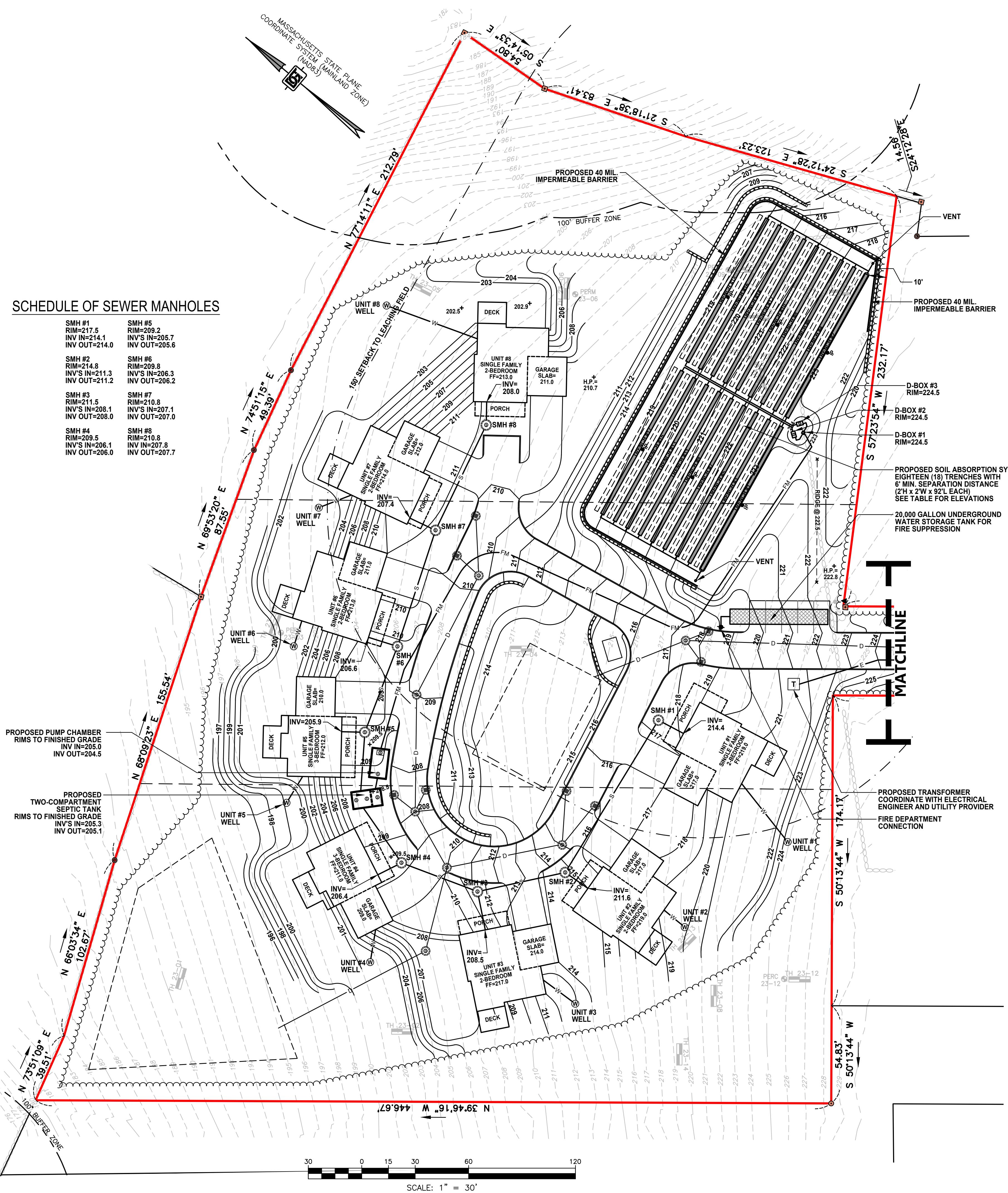
C-10

SCALE: 1" = 30'

PROJECT NO.:
F-25889

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SCHEDULE OF ELEVATIONS

FIRST FLOOR ELEVATIONS	= SEE PLAN
PIPE INVERT AT FOUNDATIONS	= SEE PLAN
SEPTIC TANK INLET INVERT	= 205.30
SEPTIC TANK OUTLET INVERT	= 205.10
PUMP CHAMBER INLET INVERT	= 205.00
PUMP CHAMBER OUTLET INVERT	= 204.50
DISTRIBUTION BOX #1 INLET INVERT	= 222.60
DISTRIBUTION BOX #1 OUTLET INVERT	= 222.40
DISTRIBUTION BOX #2 INLET INVERT	= 222.26
DISTRIBUTION BOX #2 OUTLET INVERT	= 222.05
DISTRIBUTION BOX #3 INLET INVERT	= 222.26
DISTRIBUTION BOX #3 OUTLET INVERT	= 222.05

DESIGN DATA

FLOW CALCULATIONS	
1. BUILDING USE:	RESIDENTIAL DWELLINGS
2. TWO (2) 3-BEDROOM DWELLINGS:	660 GPD
SIX (6) 2-BEDROOM DWELLINGS:	1,320 GPD
3. DESIGN FLOW - TITLE 5:	110 GPD/BEDROOM
4. TOTAL DAILY FLOW - TITLE 5:	1,980 GALLONS

SYSTEM REQUIREMENTS	
1. DESIGN PERCOLATION RATE:	47 MINUTES PER INCH
2. SOIL CLASS:	CLASS III
3. LONG TERM ACCEPTANCE RATE (LTAR):	0.20 GAL/SF
4. TOTAL AREA REQUIRED - TITLE 5:	9,900 SF
5. TOTAL AREA PROVIDED:	9,936 SF

SEPTIC TANK CALCULATIONS	
VOLUME COMPARTMENT #1 (200% DAILY FLOW):	3,960 GALLONS
VOLUME COMPARTMENT #2 (100% DAILY FLOW):	1,980 GALLONS
MINIMUM REQUIRED SEPTIC TANK VOLUME:	5,940 GALLONS
PROVIDED SEPTIC TANK #1 (2 COMPARTMENT):	6,000 GALLONS

NITROGEN LOADING LIMITATIONS (310 CMR 15.214)
222,696 SF / 40,000 SF = 5.5674 ACRES
5.5674 ACRES x 440 GPD PER ACRE = 2,450 GPD
2,450 GPD > 1,980 GPD -- OKAY

PRIMARY TRENCH DESIGN DATA

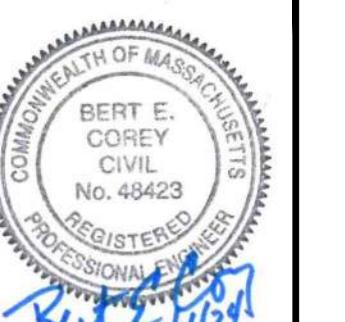
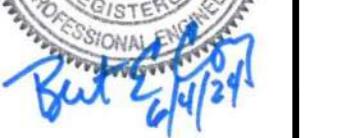
TRENCH NO.	1 & 2	3 & 4	5 & 6	7 & 8	9 & 10	11 & 12	13 & 14	15 & 16	17 & 18
MAXIMUM FINISH GRADE	225.35	224.85	224.35	223.35	222.85	222.35	221.85	221.35	221.35
MINIMUM FINISH GRADE	223.35	222.85	222.35	221.85	221.35	220.85	220.35	219.85	219.35
BREAKOUT ELEVATION AT BEGINNING	222.35	221.85	221.35	220.85	220.35	219.85	219.35	218.85	218.35
BREAKOUT ELEVATION AT END	221.90	221.40	220.90	219.90	219.40	218.90	218.40	217.90	217.90
INVERT ELEVATION AT BEGINNING	221.85	221.35	220.85	220.35	219.85	219.35	218.85	218.35	217.85
INVERT ELEVATION AT END	221.40	220.90	220.40	219.90	219.40	218.90	218.40	217.90	217.40
BOTTOM OF STONE ELEVATION	219.40	218.90	218.40	217.90	217.40	216.90	215.90	215.40	215.40
DESIGN GROUNDWATER ELEVATION	215.16	214.59	214.02	213.45	212.82	211.94	211.43	210.63	209.90

RESERVE TRENCH DESIGN DATA

TRENCH NO.	1 & 2	3 & 4	5 & 6	7 & 8	9 & 10	11 & 12	13 & 14	15 & 16	17 & 18
BOTTOM OF STONE ELEVATION	219.14	218.64	218.14	217.64	217.14	216.64	216.14	215.64	215.14
DESIGN GROUNDWATER ELEVATION	214.86	214.29	213.73	213.12	212.49	211.82	211.02	210.22	209.63

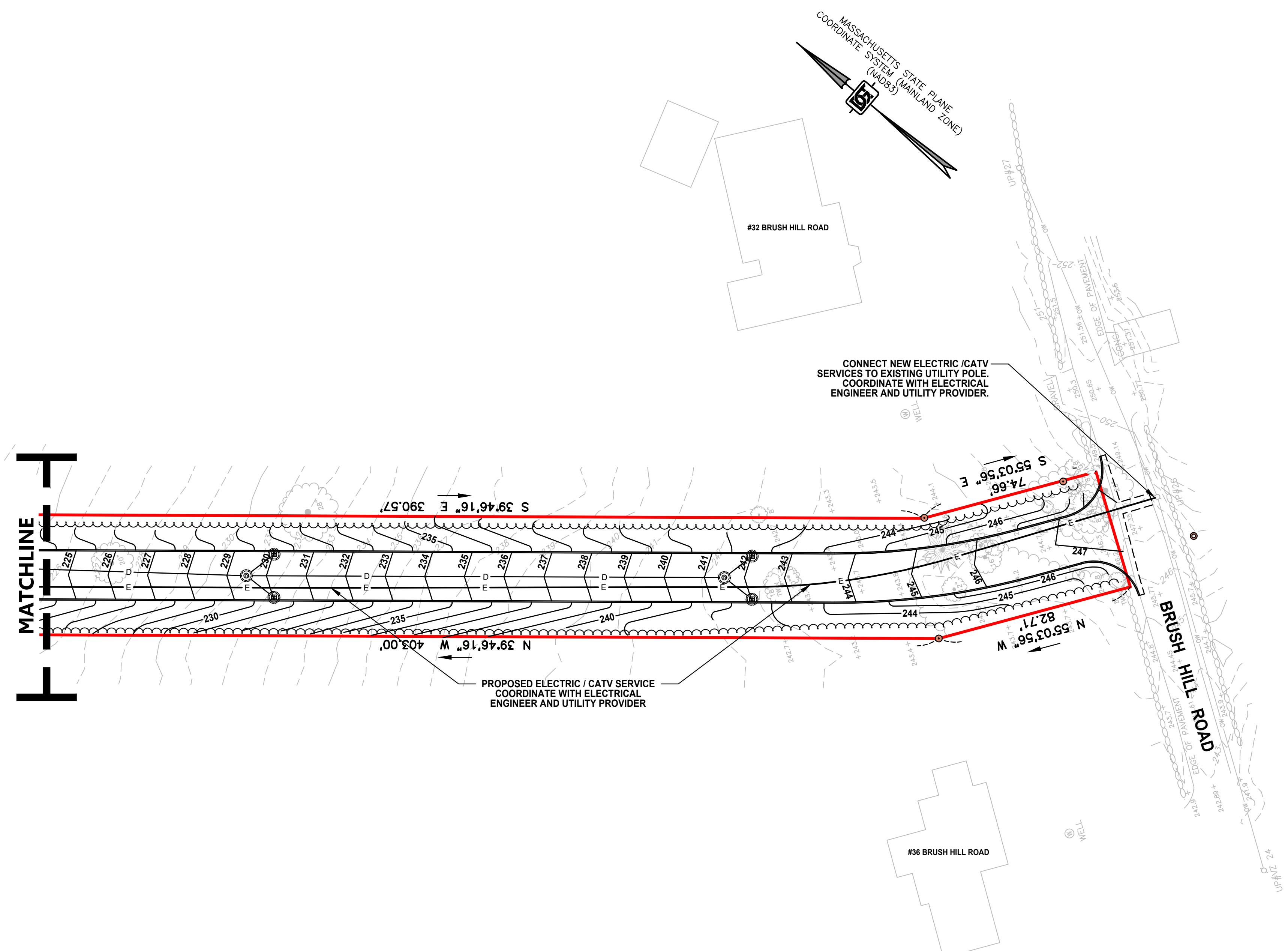
GENERAL NOTES

- SEE SHEET C-2 FOR SETBACK DISTANCES TO ABUTTING WELLS.

F1 F-25860 F-25869 Fomc 34 Brush Hill Rd Sherborn MA Correspondence Sent 2024-06-04 to Client (Comp Permit 402) F-25869 Site Plan.dwg										
PARCEL ID:	MAP 1, LOT 0, BLOCK 18									
ISSUED FOR:	COMPREHENSIVE PERMIT APPLICATION									
 										
NO. APP	DATE	DESCRIPTION								
DATE: JUNE 4, 2024										
SCALE: 1" = 30'										
DESIGN:	KMR	DRAFTED:	KMR	CHECKED:	BEC	PROJECT TITLE:				
SHERBORN COTTAGE COURT										
34 BRUSH HILL ROAD SHERBORN, MA 01770										
SHEET TITLE:										
SITE UTILITIES PLAN - 01										
SHEET: 11 OF 12										
PROJECT NO.: F-25869										
C-11										

LEGEND

- 123
- D
- RD
- PROPOSED CONTOUR ELEVATION
- PROPOSED DRAIN LINE
- PROPOSED ROOF DRAIN
- PROPOSED DRAIN MANHOLE
- PROPOSED STORMWATER TREATMENT UNIT
- PROPOSED DEEP SUMP CATCH BASIN
- PROPOSED AREA DRAIN
- PROPOSED OUTLET CONTROL STRUCTURE
- PROPOSED SEWER MANHOLE
- HDPE
- +123.4
- T.C.=123.4
- B.C.=123.4
- T.W.=123.4
- PROPOSED TOP OF CURB ELEVATION
- PROPOSED BOTTOM OF CURB ELEVATION
- PROPOSED TOP OF WALL ELEVATION
- PROPOSED BOTTOM OF WALL ELEVATION



LEGEND

123	PROPOSED CONTOUR ELEVATION
D	PROPOSED DRAIN LINE
RD	PROPOSED ROOF DRAIN
DMH (D)	PROPOSED DRAIN MANHOLE
STU (D)	PROPOSED STORMWATER TREATMENT UNIT
CB	PROPOSED DEEP SUMP CATCH BASIN
AD	PROPOSED AREA DRAIN
OCS (D)	PROPOSED OUTLET CONTROL STRUCTURE
SMH (S)	PROPOSED SEWER MANHOLE
HDPE	HIGH DENSITY POLYETHYLENE
+123.4	PROPOSED SPOT ELEVATION
T.C.=123.4	PROPOSED TOP OF CURB ELEVATION
B.C.=123.4	PROPOSED BOTTOM OF CURB ELEVATION
T.W.=123.4	PROPOSED TOP OF WALL ELEVATION
B.W.=123.4	PROPOSED BOTTOM OF WALL ELEVATION

**344 BRUSH HILL ROAD
SHERBORN, MA 01770**

HEET TITLE:

SITE UTILITIES

PLAN - 02

**34 BRUSH HILL ROAD
SHERBORN, MA 01770**

SITE UTILITIES

177

EE1:
OF 12

C-12

ACT NO.:

ST NO..
5889

SCALE: 1" = 30'